

**PART 5: Planning Applications for Decision**

**Item 5.4**

**1 APPLICATION DETAILS**

Ref: 22/03139/FUL  
 Location: Crossways Dental Practice, 328 Coulsdon Road, Coulsdon CR5 1EB  
 Ward: Old Coulsdon  
 Description: Erection of a part 1, part 2 storey detached dwelling to the rear of 328 Coulsdon Road, accessed from The Crossways, with car parking, and external bin and bike stores  
 Drawing Nos: PD-0302 A1/01; A1/02B; A1/03C; A1/04C; A1/05C; A1/06, A1/07  
 Applicant: Mr Patel, Crossways Dental Practice  
 Agent: Carl Pringle  
 Case Officer: Yvette Ralston

	1b2p	2b3p	3b4p	TOTAL
<b>Existing</b>				0
<b>Proposed</b> (Market Housing)	0	0	1	1

<b>Vehicle and Cycle Parking (London Plan Standards)</b>	
<b>PTAL: 2</b>	
<b>Car Parking maximum standard</b>	<b>Proposed</b>
1	1
<b>Long Stay Cycle Storage minimum</b>	<b>Proposed</b>
2	2

1.1 This application is being reported to sub-committee because:

- Referral to committee by ward Councillor Cllr Nikhil Sherine Thampi

**2 RECOMMENDATION**

- 2.1 That the Committee resolve to GRANT planning permission
- 2.2 That the Director of Planning and Sustainable Regeneration is delegated authority to issue the planning permission.
- 2.3 That the Director of Planning and Sustainable Regeneration is delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

**Conditions**

- 1) Commencement time limit of 3 years
- 2) Carried out in accordance with the approved drawings

Pre-commencement (including demolition)

- 3) Submission of Construction Logistics Plan including agreement of any potential impacts on bus stop with TfL

Prior to above ground works

- 4) Submission of materials/details
- 5) Submission of final cycle and refuse storage details
- 6) Submission of landscaping details including boundary treatments
- 7) Submission of surface water drainage strategy

Prior to occupation

- 8) Installation of car parking, and visibility splays as shown on plan
- 9) Submission of details of any external energy generation measures (ASHP, PVs etc) including noise control where necessary

Compliance

- 10) Obscure glazing on the first floor window on the east elevation
- 11) Delivery of M4(2) 'accessible and adaptable' dwelling
- 12) No illuminations that would cause nuisance to local residents
- 13) Compliance with Fire Statement
- 14) Water efficiency
- 15) Removal of PD rights

**Informatives**

- 1) Community Infrastructure Levy
- 2) Code of practice for Construction Sites
- 3) Compliance with Building/Fire Regulations
- 4) Construction Logistics Informative
- 5) Highways informative in relation to s278 and s38 works required
- 6) Any other informative(s) considered necessary by the Director of Planning and Sustainable Regeneration

- 2.4 That the Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

**3 PROPOSAL AND LOCATION DETAILS**

**Proposal**

- 3.1 The proposal is for a part 1, part 2 storey detached dwelling to the rear of 328 Coulsdon Road, accessed from The Crossways, with car parking, and external bin and bike stores.

**Site and Surroundings**

- 3.2 The site is located on a corner on west side of Coulsdon Road and the north side of The Crossways. The host building on the site is a semi-detached 2 storey building in use as a dentist at the ground floor with 2 flats above. The rear of the site comprises a small garage and space for car parking. There is a vehicular access into the site from The Crossways. There is also a bus stop directly outside the site on The Crossways.

- 3.3 The surrounding area comprises predominantly 2 storey semi-detached dwellings. There are no land use restrictions on the site. The site has a PTAL rating of 2. It is at low risk of surface water flooding.

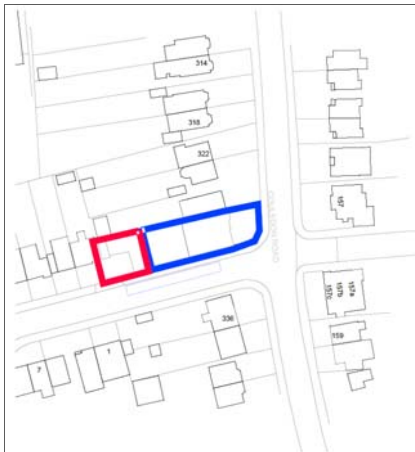


Figure 1: Site location plan



Figure 2: Aerial view

### Planning Designations and Constraints

- 3.4 The site is subject to the following formal planning constraints and designations:

- PTAL: 2
- Coulsdon Road is part of the strategic road network managed by TfL
- Flood Risk Zone: 1
- Surface water flood risk: low
- Archaeological Priority Area

### Planning History

- 3.5 99/02932/P: Demolition of existing rear lean-to; erection of single storey side extension and conservatory at rear – permission granted 01.03.2000
- 3.6 02/01575/P: Use of ground floor as dental surgery and first floor as flat; erection of first floor side extension – permission granted 24.07.2002
- 3.7 18/04602/FUL: Alterations, erection of first floor side and single/two storey rear extension, erection of dormer extension on rear roof slope, use of part of extension as 1 bedroom flat, use of part of extension for the existing dentist and use of part of extension for the existing residential unit – permission granted 12.04.2019. This permission has been implemented.
- 3.8 20/05731/CONR: Variation of Condition 1 (approved plans) attached to planning permission 18/04602/FUL for Alterations, erection of first floor side and single/two storey rear extension, erection of dormer extension on rear roof slope, use of part of extension as 1 bedroom flat, use of part of extension for the existing dentist and use of part of extension for the existing residential unit – permission refused 10.02.2021
- 3.9 23/03469/ADV: One freestanding illuminated "V" advertisement board on posts, an illuminated advertisement board fixed to the premises at front and a

non illuminated advertisement boarded fixed to the side of the premises. – pending consideration

3.10 24/00340/FUL: Installation of three air conditioning units to the ground floor rear elevation. [Retrospective application]. – permission refused 20.03.2024 on the following grounds:

- Failure to demonstrate that the air conditioning units would not harm the residential amenities of nearby residential properties by reason of noise and general disturbance
- Inaccurate/ambiguous plans

3.11 24/02190/FUL: Installation of two air conditioning units and one air source heat pump to the rear elevation at ground floor level, two air conditioning units to the front elevation at ground floor level, and one rooflight to the front roofslope [retrospective application]. – permission refused 16.08.2024 on the following ground:

- Failure to demonstrate that the air conditioning units would not harm the residential amenities of nearby residential properties by reason of noise and general disturbance

3.12 There is an open enforcement case (ref: 23/00298/DEV) regarding these air conditioning units. The investigation is ongoing. It does not alter the recommendation made within this report.

#### Pre-application history

3.13 21/00440/PRE: Erection of detached three bedroom single storey dwelling with accommodation within the roof.

3.14 Following this pre-application, revised plans and an updated parking stress survey was received. These have been incorporated as part of this application and are assessed accordingly in the main body of this report.

## **4 SUMMARY OF KEY REASONS FOR RECOMMENDATION**

- The proposed new dwelling is of a scale and design that complements the appearance of the area and the surrounding dwellings.
- The proposed new dwelling would provide a good quality 3-bed unit, contributing to the Council's need for family sized accommodation.
- Amenity impacts on neighbouring properties are found to be acceptable.
- There is no objection to the change of use of the rear part of the garden which is shared between the dentist and the flats above.
- The parking provision of 1 space on-site has been assessed and justified and is acceptable.

4.1 The following sections of this report summarise the officer assessment and the reason for the recommendation.

## **5 CONSULTATION RESPONSE**

Greater London Archaeological Advisory Service (GLAAS)

- Site is located within a Tier III APA. No further assessment or conditions are necessary.

### Transport for London (TfL)

- TfL has no strategic comments to make on the application.
- The development should comply with transport policies set out in the London Plan
- The Crossways supports 3 bus routes. In the event that implementation of the development requires the temporary re-routeing of bus services or temporary or permanent closure or alteration of a bus stop or shelter or other such arrangements, these must be agreed with TfL before the work commences.

### **Internal consultees**

#### Strategic Transport

- The proposal would result in the loss of existing parking provision and a garage. 1 new unit with 1 parking space is policy compliant in this PTAL 2 location. The proposed removal of 2 vehicle movements at the entrance point could be considered an improvement in terms of highway impacts.
- The parking stress assessment indicates there is on street parking available, demonstrating that the loss of parking would not have a detrimental impact upon the highway network.
- Cycle store should have space for a wider or adapted bike.
- Cycle parking should be provided for the dental surgery.  
*Officer response: This is provided as part of approved planning application ref: 18/04602/FUL*
- Conditions required regarding bins, no obstructions above 0.6m in height within the sightlines, CLP condition, S78 required.  
*Officer response: all other points are addressed in the report or will be finalised by condition.*

## **6 LOCAL REPRESENTATION**

- 6.1 A total of 8 neighbouring properties were initially notified about the application and invited to comment.
- 6.2 A mid-application meeting (ref: 21/00440/PRE) took place with the applicant in January 2024. Revised plans and an updated parking stress survey were received in May 2024. A public re-consultation took place between 24/07/24 and 16/08/24.
- 6.3 The total number of representations received from neighbours and local groups in response to notification and publicity of the application (both public consultations) were as follows:
- No of individual responses: 12   Objecting: 11   Supporting: 1
- 6.4 The following issues were raised in representations that are material to the determination of the application, and they are addressed in the next section of this report:

<b>Representation</b>	<b>Officer's response</b>
<b>Character and appearance</b>	
Overdevelopment, site small, unsuitable for housing	The site has a street-facing frontage and complies with policy DM10.4e regarding backland development. This is discussed in further detail in this report.
Overbearing	The proposed dwelling is part 1, part 2 storeys in height so would not be described as overbearing. This is discussed in further detail in the 'character' and 'neighbouring amenity' sections of this report.
Detrimental to the appearance of the Crossways, not in keeping with neighbouring dwellings.	The proposed development is of a scale, bulk and design that is compatible with the appearance of the area and surrounding buildings.
Host property already been extended in height and width to the boundary and into the garden.	Noted. This has been taken into account in the assessment of this application.
<b>Parking and highways impacts</b>	
Parking stress survey is from 2018, which is out of date.	The parking stress survey has been updated (February 2024).
There is congestion on the road because of parking for the dentist and school.	This is addressed in the 'transport' section of this report.
3 parking spaces will be displaced onto the road. More parking on the road will cause an obstacle to buses.	This is addressed in the 'transport' section of this report.
Development here will make pulling onto the road more dangerous.	There would be an overall reduction in the number of vehicular movements due to the reduction in the number of parking spaces in this location from 2 to 1. Access arrangements would remain the same.
There is no dedicated parking for the dentist. This space should be used for dentist parking	This is not proposed and the application as submitted must be assessed on its merits.
There is a bus stop outside the site which would be obstructed	The applicant has discussed the application with TfL and they have been consulted as part of the application process. TfL has raised no objection regarding the bus stop.
The ULEZ covers adjacent roads which has increased demand for parking on The Crossways	This has been tested in the updated Transport Technical Note
There will be need for clinical waste collection as well as general waste and recycling	The applicant has confirmed that Dentist waste will be collected from the front of the site on Coulsdon Road.

<b>Neighbouring amenity impacts</b>	
Loss of light to stairwell/landing window of 2 The Crossways	Addressed in the report.
Overbearing and loss of light	Addressed in the report.
Overlooking to 324 Coulsdon Road, reducing privacy	Addressed in the report.
1 The Crossways will have difficulty reversing from their driveway	No changes to the access arrangements are proposed. The relationship between 1 The Crossways and the application site will be no different to the relationship between dwellings/driveways facing each other along the rest of the Crossways.
<b>Quality of accommodation</b>	
Cramped and unattractive to a family due to its small size and limited outside space.	The proposed dwelling has 3 bedrooms so could be used by a family.
Cramped accommodation and insufficient leisure space	The proposed dwelling complies with space standards for internal and external space. Addressed in the 'quality of accommodation' section of this report.
Insufficient space to install a heat pump if required in the future	There is space to install a heat pump and this is permitted development.
<b>Other matters</b>	
Construction impacts	A Construction Logistics Plan will be required by condition.
Lack of infrastructure. Additional residents will cause strain to community facilities, especially schools and health facilities.	A CIL contribution towards local infrastructure will be required.
Residents objections have not been listened to. The changes do not address our concerns.	All residents' objections have been taken into account. The application has been found to accord with planning policy.
The extensions to the host property already take up most of the 50% PD rights.	This is not relevant as this is a full planning application, not an LDC.
Inconsistencies in the drawings and visuals.	Drawings have been updated again to resolve any inconsistencies (uploaded 24/07/24). The visuals are for information purposes only.
Pre-app advice has not been adhered to e.g. the house has not reduced in size and the landscaping is not improved.	Following the pre-app, minor external design changes have taken place. Updates were also made to the transport information. Landscaping will be finalised by condition.
There will be serious difficulties getting large vehicles/skip etc into the site for construction purposes.	These details will be addressed as part of a detailed Construction Logistics Plan, which will also require liaison with TfL with regard to impacts on the bus stop.

6.5 Cllr Nikhil Sherine Thampi (Old Coulsdon ward) objects to the application on the following grounds, and has referred the application to Planning Committee for consideration on the following grounds:

- Garden development; Over crowding; Out of character as the proposed developments building's front line is in the front of the next door at 2.  
*Officer response: These observations are addressed in the report.*

6.6 The Old Coulsdon Residents Association has objected on the following grounds:

- Overdevelopment. Poor leisure space, not enough to install a heat pump if required by government legislation. Cramped accommodation.
- Too close to neighbours, overlooking.
- Existing extension to surgery already takes up a high percentage of the garden.
- Also residents have expressed concern that the extension and this proposal were kept separate.
- None of these applications extend the business of the Dentists surgery or enable better service of this operation to residents.  
*Officer response: The dentist surgery remains operational. All other points are addressed in this report.*

6.7 The East Coulsdon Residents Association has objected on the following grounds:

- Planners stated in the RA-Planners meeting that they were against backland development. This is such a development.  
*Officer response: This site is a corner plot with a street-facing frontage so would not be considered traditional backland development. The proposal complies with policy DM10.4e regarding retained garden space.*
- Previous developments to the host property have already taken up garden land  
*Officer response: This is taken into account in the assessment of this application.*
- The building line is in front of the house next door at 2 The Crossways and abuts the boundary of 326 Coulsdon Road leading to overlooking.  
*Officer response: The front building line is in line with 2 The Crossways, then steps forward by 1m to add some variation to the frontage*
- Query as to why the application was submitted in July 2022 but only validated in September 2023.  
*Officer response: The application was first received in July 2022 but was invalidated awaiting further information. Further information was received in September 2023 so the application was validated on 09.09.2023. Further delays followed due to the mid-application meeting and the subsequent amendments to the plans.*

6.8 The following comments were received in support of the application:

- A good addition to the street
- Good to see family sized dwelling rather than block of flats



- Construction impacts will need to be carefully considered because of the bus stop and school.

## 7 RELEVANT PLANNING POLICIES AND GUIDANCE

### Development Plan

7.1 The Council's adopted Development Plan consists of the London Plan (2021), the Croydon Local Plan (2018) and the South London Waste Plan (2022). Although not an exhaustive list, the policies which are most relevant to the application are:

#### London Plan (2021)

- D1 London's form, character and capacity growth
- D3 Optimising site capacity through the design led approach
- D4 Delivering Good Design
- D5 Inclusive Design
- D6 Housing quality and standards
- D7 Accessible housing
- D12 Fire Safety
- H1 Increasing housing supply
- H2 Small sites
- G7 Trees and Woodlands
- SI 2 Minimising Greenhouse Gas Emissions
- SI 13 Sustainable Drainage
- T4 Assessing and mitigating transport impacts
- T5 cycling
- T6 car parking
- T6.1 Residential parking

#### Croydon Local Plan (2018)

- SP2 Homes
- SP4 Urban Design and Local Character
- SP6 Environment and Climate Change
- SP8 Transport and communications
- DM1 Housing Choice for Sustainable Communities
- DM10 Design and Character
- DM13 Refuse and Recycling
- DM23 Development and Construction
- DM25 Sustainable Drainage Systems and Reducing Flood Risk
- DM27 Biodiversity
- DM28 Trees
- DM29 Promoting Sustainable Travel and Reducing Congestion
- DM30 Car and cycle parking in new development

7.2 The Development Plan should be read as a whole, and where policies conflict with each other, the conflict must be resolved in favour of the policy contained in the last document to be adopted, approved or published as part of the development plan, (in accordance with s38(5) of the Planning and Compulsory Purchase Act 2004).

## **Planning Guidance**

### National Planning Policy Framework (NPPF)

7.3 Government Guidance is contained in the NPPF, updated on 20 July 2021, and accompanied by the online Planning Practice Guidance (PPG). The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Delivering a Sufficient Supply of Homes
- Promoting Sustainable Transport
- Achieving Well Designed Places

### SPDs and SPGs

7.4 There are also several Supplementary Planning Documents (SPD) and Supplementary Planning Guidance (SPG) documents which are material considerations. Although not an exhaustive list, the most relevant to the application are:

- London Housing SPG (March 2016)
- Technical Housing Standards: Nationally Described Space Standard (2015)
- National Design Guide (2021)
- Housing Design Standards LPG (2023)

## **8 MATERIAL PLANNING CONSIDERATIONS**

8.1 The main planning issues raised by the application that the committee must consider are:

1. Principle of development
2. Impacts on the character and appearance of the site and surrounding area
3. Amenity impacts on neighbouring properties
4. Quality of accommodation
5. Transport and highways impacts
6. Impact on trees and landscaping
7. Sustainability and flooding impacts

### **Principle of development**

8.2 The host property is a dentist with 2 flats above. The land at the rear, where the application site is located, is a shared garden that is used by both flats and the dentist (confirmed within the Officer report for application ref: 18/04602/FUL). Only the dentist has direct access to the rear garden whilst the residents of the upper floor flats have to enter the garden via a side gate on the Crossways. The proposal would result in the loss of 200sqm of shared garden land (some of which is hardstanding/parking/garage space), leaving a total of 122sqm of shared garden land for the host property.

8.3 The area at the rear where the application site is located has a derelict shed and 2 staff parking spaces for the dentist surgery. This land was unused at the time of both site visits. Local Plan policy DM19 should be considered with regards to loss of

dentist land. This policy states that the loss of community facilities will be permitted if it can be demonstrated that there is no need for the existing premises or land for a community use and that it no longer has the ability to serve the needs of the community. The land at the rear of the site is not essential for the continued operation of the dentist so it can be concluded that there is no conflict with Local Plan policy DM19. The loss of the 2 car parking spaces is assessed elsewhere in this report. There is no in principle objection to the loss of the rear part of the shared garden land.

- 8.4 Local Plan Policy SP2 states that the Council will apply a presumption in favour of development of new homes provided applications for residential development meet the requirements of other applicable policies. Policy SP2.2 sets out that the Council seeks to deliver 32,890 homes over the 20-year period between 2016 and 2036. The proposal for 1 new dwelling would make a minor contribution towards the Council's housing targets and is acceptable in principle subject to compliance with other material considerations, discussed throughout this report.
- 8.5 The proposal is for a 3b4p dwelling which would contribute towards the Council's needs for family sized homes as outlined in Local Plan policy SP2.7.

#### **Impacts on the character and appearance of the site and surrounding area**

- 8.6 Policy SP4.1 and DM10.1 of the Croydon Local Plan require development to be of a high quality, which respects and enhances Croydon's varied local character and contributes positively to public realm, landscape and townscape to create sustainable communities. Proposals should respect the development pattern, layout and siting, the scale, height, massing, density and appearance, existing materials and built and natural features of the surrounding area. London Plan Policy D3 seeks to ensure that a design-led approach is followed and seeks to optimise site capacity by ensuring that development is of the most appropriate form and land use for the site.
- 8.7 The proposal involves sub-dividing the site and utilising the rear part of the site for the development of 1 new house. Local Plan Policy DM10.4e states that in the case of development in the grounds of an existing building which is retained, a minimum length of 10m and no less than half or 200m<sup>2</sup> (whichever is the smaller) of the existing garden area must be retained for the host property. The existing garden is approximately 240sqm in area (factoring in the extensions to the host dwelling that have already been constructed as approved under application ref: 18/04602/FUL, but not including the parking area and hardstanding at the rear, where the application site is located as these which do not form a useable part of the shared garden area). Following the proposed development, the host dwelling would retain a garden of 10m in length and 122sqm in area, which complies with the policy requirement for backland development. The boundary treatment separating the application site from the host dwelling would be a 1.8m high fence.

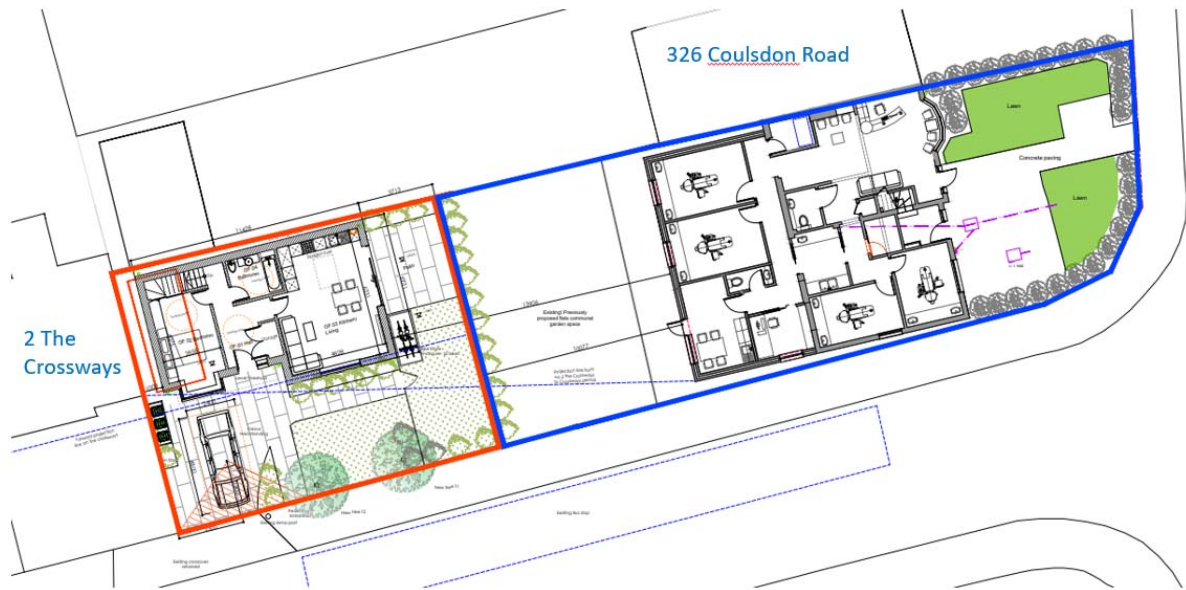


Figure 3: Proposed site layout plan

8.8 The application site is on a corner, so the proposed new dwelling has a street facing frontage so would not be considered traditional 'backland development'. The front building line adjacent to the neighbouring dwelling at number 2 The Crossways would correspond with the front of number 2, and would then step forwards by 1m on the east side. This stepped footprint forms a successful bookend to the end of the terrace and provides a transition between number 2 The Crossways and the host dwelling which has its side elevation much closer to the pavement. The separation distance to number 2 would be 3.4m measured between the two side elevations at first floor level. This is consistent with separation distances between other pairs of semi-detached dwellings along The Crossways and is acceptable.

8.9 The proposed dwelling would be part 2 storeys, part single storey. The height of the 2 storey element would match the eaves line of number 2 The Crossways and the ridge line would be below that of number 2. The height would step down to single storey towards the host dwelling. The pitch of the roof would reflect that of neighbouring dwellings. The scale and form of the proposed dwelling is a good response to the site's context. A condition would be attached to remove PD rights for the enlargement of the new dwelling to ensure that any potential future amendments to the scale and appearance of the dwelling can be assessed by the LPA in full.

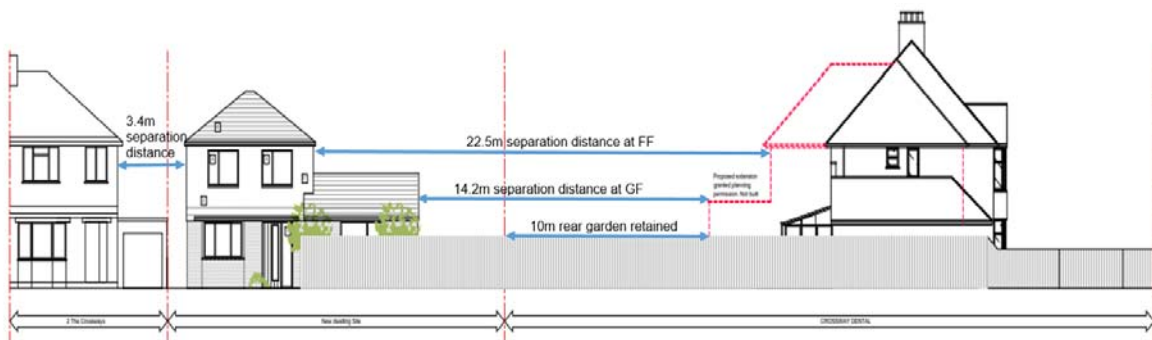


Figure 4: Proposed streetscene along the Crossways

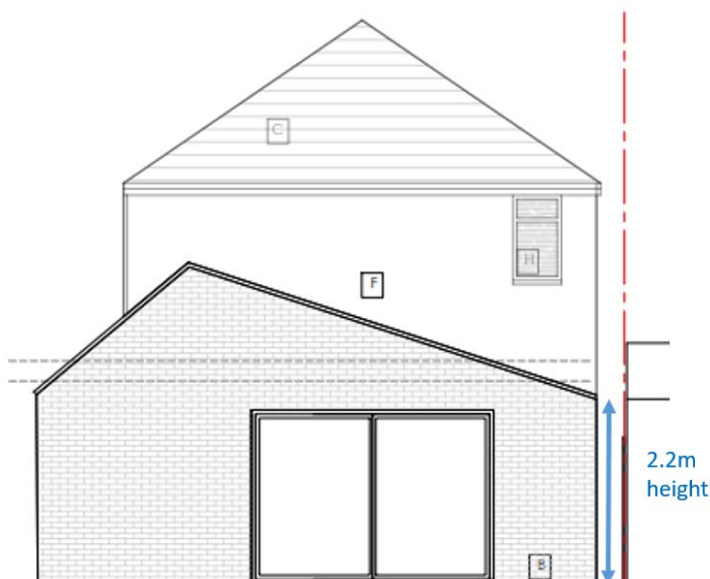
8.10 Amendments have been made to the appearance of the dwelling following pre-application discussions (ref: 21/00440/PRE). The proposed dwelling would be finished with brown brick at ground floor level, white render at first floor and brown hung tile at roof level. The proposed materials reflect surrounding dwellings which generally feature the aforementioned materials in various placements. A flat projecting roof canopy is proposed above the recessed element on the front elevation to create a covered entranceway. A projecting bay window is proposed at ground floor level. Other properties along The Crossways have various front projections comprising porches or bay windows. Inset windows are proposed with grey frames. These features and materials are reflective of the surrounding area and the overall appearance of the dwelling is acceptable from a character point of view. Final details of materials and openings will be required by condition.

### **Amenity impacts on neighbouring properties**

8.11 In line with Policy DM10.6 new developments should avoid harm to neighbouring amenity in regard to privacy, outlook, daylight, sunlight, and noise and disturbance. Site layouts and new buildings should be orientated and designed to minimise amenity impacts where possible, with residual impacts designed out. The closest residential dwellings are 326 Coulsdon Road to the north and 2 The Crossways to the west. Amenity impacts on the host dwelling must also be considered.

#### 326 Coulsdon Road

8.12 326 Coulsdon Road is the adjoining semi-detached dwelling to the north of the application site. It is a 2 storey dwelling with a rear and side roof dormer and a single storey rear extension. There is an outbuilding at the rear of the garden which it is understood is used as office space. The proposed dwelling would be part 1, part 2 storeys in height and would be sited close to the boundary (approximately 0.3m gap). The 2 storey element would be located adjacent to the outbuilding, and the building would step down to single storey where it is adjacent to the private garden space of number 326. The single storey element would be 2.2m in height to the eaves, against a 1.8m high boundary fence, and the asymmetrical roof would pitch gently away from the fence to keep the height as low as possible and mitigate overbearing impacts on the garden of number 326.



*Figure 5: Proposed side elevation (east) showing the gentle pitch of the roof of the single storey element away from the garden of no. 326 Coulsdon Road*

- 8.13 The orientations, with the application site being located to the south, mean that the rear garden of number 326 may experience some overshadowing. This has not been formally tested, however the BRE guideline states that for a garden to appear adequately sunlit throughout the year, at least 50% of an amenity area should receive at least 2 hours of sunlight on 21st March. Given the size of the garden and the low height of the proposed dwelling, the mass based on officer's experience of such tests would not lead to a breach of this guideline and the garden of number 326 would continue to receive a policy compliant level of sunlight. Regard has also been given to the fact that the proposed new dwelling would mostly impact the rear garden area, rather than the garden area directly adjacent to the rear elevation of number 326.
- 8.14 The proposed new dwelling would not have any windows on its rear (north) elevation so would not lead to overlooking. The skylight at the rear would not be orientated towards the garden of number 326.

## 2 The Crossways

- 8.15 2 The Crossways is the neighbouring dwelling to the west. It is a 2 storey semi-detached dwelling separated from the application site by a garage. The proposed 2 storey element of the new dwelling would have a ridge height lower than that of number 2, and the separation distance is acceptable, so the proposed new dwelling would not be overbearing. Number 2 has a side facing window serving a staircase/hallway. This window would receive some reduction in light and outlook, however the window does not serve a habitable room (such as a living room or bedroom) so this would not form a reason for refusal of the application.
- 8.16 The stepping forward of the front building line of the proposed new dwelling would not breach a 45 degree line drawn in plan from the front windows of number 2 so would have no impact on daylight to these windows. No side facing windows are proposed facing number 2 so no overlooking issues are raised. The new dwelling would have no impact on overshadowing of the garden of number 2 the Crossways.

## 328 Coulsdon Road (host building)

- 8.17 The host dwelling has a recent ground floor rear/side extension. The ground floor is used as dental surgeries and a staff room, and upper floors are residential. The proposed separation distance to the host dwelling would be 14m at ground floor level and 22.5m at first floor level. Guidance in the supporting text to Local Plan policy DM10 (para 6.80) indicates that 18-21m is a good practice 'yardstick' for separation distances between facing windows in order to maintain visual separation. At first floor level, the proposed new dwelling would only have 1 small window facing east and this serves a bathroom so would be obscured by condition so would not result in any overlooking issues.
- 8.18 Flats within the upper floors of the host building would have a clear view towards the proposed new dwelling, however the separation distance is 22.5m which is acceptable. Overlooking towards the garden of the proposed new dwelling would be possible however this is not unusual in a suburban residential location such as this.

8.19 The proposed dwelling would result in the loss of some of the shared garden land for the dentist and upper floor flats. 122sqm of shared garden land would be retained for the host property. Each of the existing flats above the dentist are 1b2p units, so require a minimum of 5sqm private amenity space each in order to comply with London Plan policy D6. There is space for this provision within the retained shared garden.

### Summary

8.20 Amenity impacts towards the garden of number 326 Coulsdon Road have been successfully mitigated and no amenity impacts on other neighbouring properties are identified. No conflict with Local Plan policy DM10.6 is identified.

### **Quality of accommodation**

8.21 The National Design Guide states that well-designed homes should be functional, accessible and sustainable. London Plan policy D6 states that housing developments should be of a high quality and provide adequately sized rooms with comfortable and functional layouts. It sets out minimum Gross Internal Area (GIA) standards for new residential developments and requires that 75% of the GIA of each dwelling has a floor to ceiling height of over 2.5m.

8.22 The proposed 3b4p house would have a floorspace of 90sqm (a marginal reduction in floorspace following pre-app discussion) which complies with the internal space requirements (84sqm) set out in policy D6. The house would be dual aspect with its main outlook towards the front (south) and patio doors to the east. There is no direct outlook towards the north or west to protect neighbouring amenity, however the proposed dwelling is not deep in plan so would be adequately lit and roof lights are also utilised to ensure adequate light and ventilation is provided.

8.23 Internal layouts are acceptable. A large hallway, bedroom, bathroom and open plan kitchen/living room leading to the outdoor amenity space are proposed at ground floor level. 2 smaller bedrooms and a second bathroom are proposed at first floor level. A large storage cupboard of 1.4sqm is provided on the ground floor. This is below the London Plan requirement of 2.5sqm, however the dwelling exceeds the minimum space standards so there is opportunity for sufficient storage space. Sections show that the floor to ceiling height would be 2.5m.

8.24 Local Plan policy DM10.4 and London Plan policy D6 set out the standards for external private amenity space which is for 5sqm per 1-2 person unit and an extra 1sqm per occupant thereafter. The minimum requirement for a dwelling of this size would be 7sqm. Private amenity space is proposed to the east side and in front of the dwelling comprising a patio and grass area measuring around 70sqm in area in total. Adequate amenity space is proposed and it would be appropriately screened from the road and the neighbouring gardens by 1.8m high fencing. Details of the soft and hard landscaping and boundary treatments within the amenity space would be secured by condition (discussed below).

### Accessibility

8.25 London Plan Policy D7 requires that all new dwellings meet Building Regulations M4(2) standards. In order to achieve M4(2) standards step-free access to the dwelling and all facilities of the site must be provided. The site is flat and step-free

access is proposed from the pavement to the front entrance and also out to the amenity space via the patio door. Bin and bike stores can also be accessed in a step free manner. There is a bathroom at ground floor. The dwelling complies with M4(2) requirements and this would be conditioned.

#### Fire safety

- 8.26 A Fire Statement has been submitted which addresses the requirements of London Plan Policy D12(A). Fire service access is via The Crossways. Integrated smoke and fire alarm detection are proposed throughout the dwelling. Fire resistant doors and partitions are proposed. Details are acceptable at this stage and final fire safety details would be agreed by Building Control.

#### **Transport and highways impacts**

- 8.27 London Plan policy T4 states, amongst other things, that development proposals should not increase road danger. Local Plan policy DM30 sets out the requirements for car and cycle parking in new developments.
- 8.28 The site has a PTAL rating of 2 which indicates poor accessibility to public transport. The site is not within a CPZ.

#### Bus stop

- 8.29 There is a bus stop directly outside the site on the Crossways. The applicant has confirmed that the bus stop would not be affected by the proposal and that no changes to the length or location of the bus stop would be required. TfL was consulted on the application and raised no objection. TfL request that, if any temporary re-routing of bus services or temporary closure or alteration of the bus stop on The Crossways is required, this must be agreed with TfL. This would be incorporated into the Construction Logistics Plan (CLP) condition so that details can be agreed and approved (if necessary) with the LPA and TfL prior to commencement.

#### Loss of dentist staff parking

- 8.30 The existing arrangement is that the rear of the application site, where the new dwelling is proposed, provides 2 x staff parking spaces for the dentist surgery (the derelict garage on the application site is not used for parking). These would be lost as a result of the proposed development, leading to potential overspill parking onto the street. A parking stress survey was initially undertaken in December 2018. The daytime survey (when dentist employees would be parking on the street) concluded an average parking stress on the street of 49.7% to 54.1% within 500m walk of the site. Between 255 and 279 vacant parking spaces were available. The addition of 2 cars onto the road would have a negligible impact on congestion in the street.
- 8.31 Representations raised valid concerns that the parking stress survey was undertaken over 5 years ago, and before the implementation of the ULEZ. The Crossways is outside of the ULEZ zone however nearby roads are inside, so parking stress is understood from objectors to have increased on The Crossways. Following the pre-application meeting, the applicant has undertaken an updated parking stress assessment in accordance with the Richmond methodology, using the same assessment times as the previous survey (8am, 10am, 12pm, 1pm, 2pm, 3pm and 4pm) on Monday 19 and Wednesday 21 February 2024. The average parking stress



on the street was concluded to be between 39% and 50%, which is slightly lower than the previous survey, and not close to being at capacity. As previously, it is concluded that the addition of 2 cars onto the street would have a negligible impact on congestion in the street.

- 8.32 An overnight parking stress survey was undertaken in 2021. The parking stress within 200m of the site was found to be 25% (31 cars parking in 124 spaces) which is low.

#### Proposed car parking and access

- 8.33 London Plan Policy T6.1 would allow a maximum of 1 space per 3-bed dwelling. Space for parking 1 car on the front forecourt is proposed. This is acceptable. The appropriate pedestrian and vehicular visibility splays are shown on plan and a condition will be attached to ensure they are retained with no obstructions within the sightlines. Vehicular access would be via the existing crossover on The Crossways (which is shared with number 2 The Crossways). No changes to the existing access are proposed.

- 8.34 A separate pedestrian access gate into the site is provided to avoid conflict between a car and pedestrians. An electric vehicle charging point is shown on plan.

#### Cycle parking

- 8.35 London Plan Policy T5 requires provision of 2 cycle parking spaces per dwelling. A cycle store is proposed externally to the east side of the dwelling behind the gate, providing space for 2 bikes. This is an acceptable arrangement. Details of the bike store would be required by condition to ensure it is of an adequate size and appearance.

#### Refuse storage

- 8.36 Local Plan Policy DM13 requires the design of refuse and recycling facilities to be treated as an integral element of the overall design. Refuse storage is proposed externally on the east side of the site, appropriately screened from the street by planting. The location is appropriate for future occupants and collection operatives who would be able to collect from the Crossways. Details of the bin enclosure would be required by condition to ensure it is of an adequate size and appearance.

- 8.37 Refuse collections for the dentist currently take place from both the front of the site on Coulsdon Road and the rear of the site on The Crossways. The applicant has confirmed that provisions are being implemented currently for all waste collection to be front the front of the property on Coulsdon Road. The bin stores for the 2 flats above are also located on the Coulsdon Road frontage. The proposal would therefore not interfere with the waste collection arrangements for the host property.

#### **Impacts on trees, landscaping and biodiversity**

- 8.38 Local Plan Policy DM28 seeks to retain and protect existing trees and vegetation. Local Plan Policy DM10.8 requires proposals to incorporate hard and soft landscaping. There are no trees of note on the site currently. The proposal involves provision of 2 new trees on The Crossways frontage, which would be an enhancement to the biodiversity value and visual amenity of the site. A small area of shrubs is proposed on the frontage to separate the pedestrian entrance from the

vehicle entrance, with the remainder of the frontage open, which reflects the open arrangement of other front forecourts on this side of The Crossways. Low level hedging within a planter is proposed in front of the dwelling, with additional hedging around the perimeter of the amenity space. Grass is also proposed, along with porous hardstanding on the parking area. Final details would be required by condition.

- 8.39 This application was submitted prior to the mandatory requirement to achieve Biodiversity Net Gain (BNG) on site (which came into effect in February 2024) so there is no requirement for this application to achieve a biodiversity net gain.

### **Flooding**

- 8.40 The site is located within flood zone 1 and at low risk of surface water flooding. London Plan Policy SI13 and Local Plan Policy DM25 require inclusion of sustainable drainage in accordance with the drainage hierarchy, aiming to achieve greenfield run-off rates. A drainage plan has been submitted proposing that the application site would share a soakaway for surface water with the host building. Permeable paving is also proposed on the forecourt. This is acceptable in principle, however full details of anticipated runoff, soakaway capacity, drainage layout etc would be required by condition.

### **Sustainability**

- 8.41 The dwelling would be required by condition to achieve a maximum water use target of 110L per head per day in line with Local Plan policy SP6. Carbon dioxide emissions are covered by Building Control legislation which supersede policy requirements, although a condition is recommended in regard to plant/photovoltaic panels etc that may be necessary to ensure compliance with the new emissions standards.

### **Conclusion**

- 8.42 The application for 1 new family-sized dwelling fronting The Crossways complies with policy regarding subdivision of sites. The proposed appearance of the dwelling is of a scale form and materiality that complements neighbouring properties and good quality accommodation would be provided. Impacts on neighbouring amenity have been mitigated by the step down from 2 storeys to single storey with an asymmetrical pitched roof, and by careful window placement to avoid overlooking. There would be a reduction in car parking on the site from 2 cars (staff parking) to 1 car for the new dwelling; this has been justified in terms of overspill parking onto the street and would reduce vehicle movements onto the street, resulting in improvements to highway safety.
- 8.43 Given the consistency of the scheme with the Development Plan and weighing this against all other material planning considerations, including the benefits and the harm outlined within this report, the proposal is considered to be acceptable in planning terms subject to the detailed recommendation set out in section 2 (RECOMMENDATION).

### **OTHER MATTERS**

- 8.44 The development would be liable for a charge under the Community Infrastructure Levy (CIL).

8.45 All other planning considerations including equalities have been taken into account. All other relevant policies and considerations, including the statutory duties set out in the Equalities Act 2010, the Human Rights Act, the Planning and Compulsory Purchase Act, and the Town and Country Planning Act, have been taken into account.