

LONDON BOROUGH OF CROYDON

REPORT:	CABINET	
DATE OF DECISION	20 November 2024	
REPORT TITLE:	Croydon Local Implementation Plan (LIP) Funding Three Year Delivery Plan (2025/26 to 2027/28)	
CORPORATE DIRECTOR / DIRECTOR:	Nick Hibberd, Corporate Director of Sustainable Communities, Regeneration and Economic Recovery Heather Cheesbrough, Director of Planning and Sustainable Regeneration	
LEAD OFFICER:	Dominic Millen, Interim Head of Strategic Transport Email: dominic.millen@croydon.gov.uk Telephone: x28341	
LEAD MEMBER:	Cllr Scott Roche, Cabinet Member Streets & Environment	
KEY DECISION?	Yes	KD Number: 1524CAB Decision incurs expenditure, or makes savings, of more than £1,000,000 or such smaller sum which the decision-taker considers is significant having regard to the Council's budget for the service or function to which the decision relates. and Decision significantly impacts on communities living or working in an area comprising two or more Wards.
CONTAINS EXEMPT INFORMATION?	No	N/A
WARDS AFFECTED:	All	

1 SUMMARY OF REPORT

- 1.1** This report sets out the proposed three year delivery plan for the period 2025/26 to 2027/28 in respect of Transport for London (TfL) Local Implementation Plan (LIP) grant funding.
- 1.2** The proposed transport programmes and schemes will deliver against the priorities in the Executive Mayor of Croydon's Business Plan and the outcomes in the regional Mayor of London's Transport Strategy.

2. RECOMMENDATIONS

For the reasons set out in the report and its appendices, the Executive Mayor and Cabinet is recommended:

- 2.1** To agree the 2025/26 to 2027/28 (three years) Local Implementation Plan Delivery Plan including programmes and schemes and attached as Appendix B, to be submitted to Transport for London to grant Local Implementation Plan (LIP) funding for 2025/26.
- 2.2** To agree to delegate authority to the Corporate Director of Sustainable Communities, Regeneration & Economic Recovery (in consultation with the Executive Mayor and the Cabinet Member for Streets and Environment), to finalise and approve the LIP three Year Delivery Plan for 2025/26 to 2027/28 for submission to TfL, making any necessary adjustments to the three year Delivery Plan following TfL's feedback and prior to final submission by the Council to TfL by 13th February 2025.
- 2.3** To agree to implement the LIP three year Delivery Plan subject to confirmation of TfL funding being granted.

3 REASONS FOR RECOMMENDATIONS

- 3.1** The proposed approach is recommended for approval because it:
- Ensures that the Council's Cabinet has oversight of the proposed delivery plan and there is political oversight as it is confirmed and moves into implementation.
 - Supports the delivery of a programme of investment in transport improvements that support the Council's priorities and the delivery of other areas of work including the Reconnected Croydon Levelling Up Fund programme.
 - Will allow significant external funding to be utilised for the benefit of Croydon's residents and businesses.

4 BACKGROUND

- 4.1** Section 159 of the Greater London Authority Act 1999 allows for Transport for London (TfL) to provide financial assistance to support the delivery of safe, integrated, efficient and economic transport facilities or services. To access TfL funding, London's local authorities are required to prepare and agree with TfL a Local Implementation Plan (LIP). The LIP sets out an evidence base for an overarching longer-term programme and informs the preparation of more detailed periodic submissions including delivery

plans covering three year periods. There are then annual submissions setting out details of schemes for each year.

- 4.2 The London Borough of Croydon’s third LIP was approved in April 2019 and is still in effect so provides the framework for ongoing TfL LIP funding submissions.
- 4.3 As set out in Section 10, the schemes set out in this report are aligned with delivering the priorities in the Executive Mayor of Croydon’s Business Plan, in particular those relating to the regeneration of Croydon town centre, securing grants and leading action to reduce carbon emissions.
- 4.4 Following the conclusion of the previous three year TfL LIP funding cycle in 2024/25, the three year delivery plan set out in this report details schemes identified for 2025/26 (as per an annual submission), as well as indicative proposals for 2026/27 and 2027/28, with details subject to further development and agreement.
- 4.5 For this cycle, London’s local authorities are required to submit their proposed delivery plans by 15th November 2024. However, given that the recommendation in this report requires the proposed LIP three year Delivery Plan to be agreed at the Cabinet meeting on 20th November 2024 and be subject to call in, TfL have been notified that the submission will be delayed until 29th November 2024.
- 4.6 Following submission of the proposed LIP three year Delivery Plan, TfL will provide feedback in early January 2025. Any amendments to the proposed LIP three year Delivery Plan following feedback from TfL, will need to be made prior to the final submission on 13th February 2025. A funding confirmation letter from TfL is expected to be sent to all London Boroughs during week commencing 13th March 2025.
- 4.7 It should be noted that this is a long term, ongoing process with TfL. LIP funding supports local authorities over several years to deliver significant transport improvements, which would otherwise be unachievable given limited Council budgets.

5 SCALE OF FUNDING

- 5.1 In line with TfL guidance, it should be assumed that the level of annual funding available will be similar to 2024/25 levels. For Croydon this equated to:

<i>TfL Programme Area</i>	<i>Croydon Allocation 2024/25 (£k)</i>	<i>Notes</i>
Safer Corridors and Neighbourhoods	1,355	Covers design and delivery schemes focused on reducing traffic speeds and volumes, as well as supporting high quality conditions for walking. Allocation is based on a formula.
Cycle Network Development	175	Focused on design and delivery of segregated cycling routes. Allocation is discretionary based on specific schemes so can scale up and down.
Bus Priority	550	Focused on design and delivery of schemes that are focused on improving

		bus network efficiency. Allocation is discretionary based on specific schemes so can scale up and down.
Cycle Parking and Training	130	Covers adult and children cycle training, as well as secure on-street cycle parking in priority areas. Base allocation is the same for all boroughs.
Total	2,210	

5.2 TfL has indicated that there will also be two new funding pots which will be discretionary with allocations based on bids:

- Borough Safer Streets – Schemes of a value between £0.5m to £5m which will have the greatest road safety benefit at the highest priority locations for road danger. Funding will be prioritised for schemes having the greatest impact and those that involve reducing speed limits.
- Better Bus Partnerships – Around three boroughs will be allocated up to £10m to deliver transformational bus infrastructure. The partnership approach will also allow for consideration of service planning and enhanced customer measures.

5.3 There can also be allocations for Principal Road Renewal and Bridge Assessment & Strengthening. Given these are determined via a separate submission and approval process, they are not considered in this report.

5.4 Whilst the Plan covers a three year period, funding is confirmed by TfL on an annual basis following agreement of an annual funding submission (commonly referred to as Form A). The submission usually occurs in October each year, with review and approval taking place between January and March the following year.

6 DEVELOPING THE THREE YEAR PLAN

6.1 In developing the three year Delivery Plan consideration has been given to:

- National, regional and local strategies, plans, policies and guidance.
- Ongoing areas of work including the delivery of cycle parking and training, healthy neighbourhoods and school streets, as well as road safety schemes.
- Time limited opportunities, notably the Reconnected Croydon programme.

6.2 The proposed interventions support the delivery of the Executive Mayor of Croydon's Business Plan and the Council's transformation programme. They also support the ambitions of the Opportunity Area and complement wider plans to improve Croydon town centre and the aims of the emerging Town Centre Regeneration Strategy.

6.3 They also meet the requirements of the current TfL LIP Guidance, which effectively forms the conditions of grant for TfL LIP funding.

6.4 Some of the key elements of this Guidance include:

- Safer Corridors and Neighbourhoods should comprise a focused set of infrastructure schemes with up to 20% of the allocation available for complementary behaviour change measures.

- Infrastructure schemes must include at least one core measure alongside supporting measures. They also need to be in suitable locations as shown in Appendix A, which sets out the schemes, measures and locations that are eligible for LIP Safer Corridors and Neighbourhoods funding.
- Programmes and schemes must be focused on priority areas identified by TfL using a data led approach. An example of this mapping in respect of road safety priority locations is included below for context.

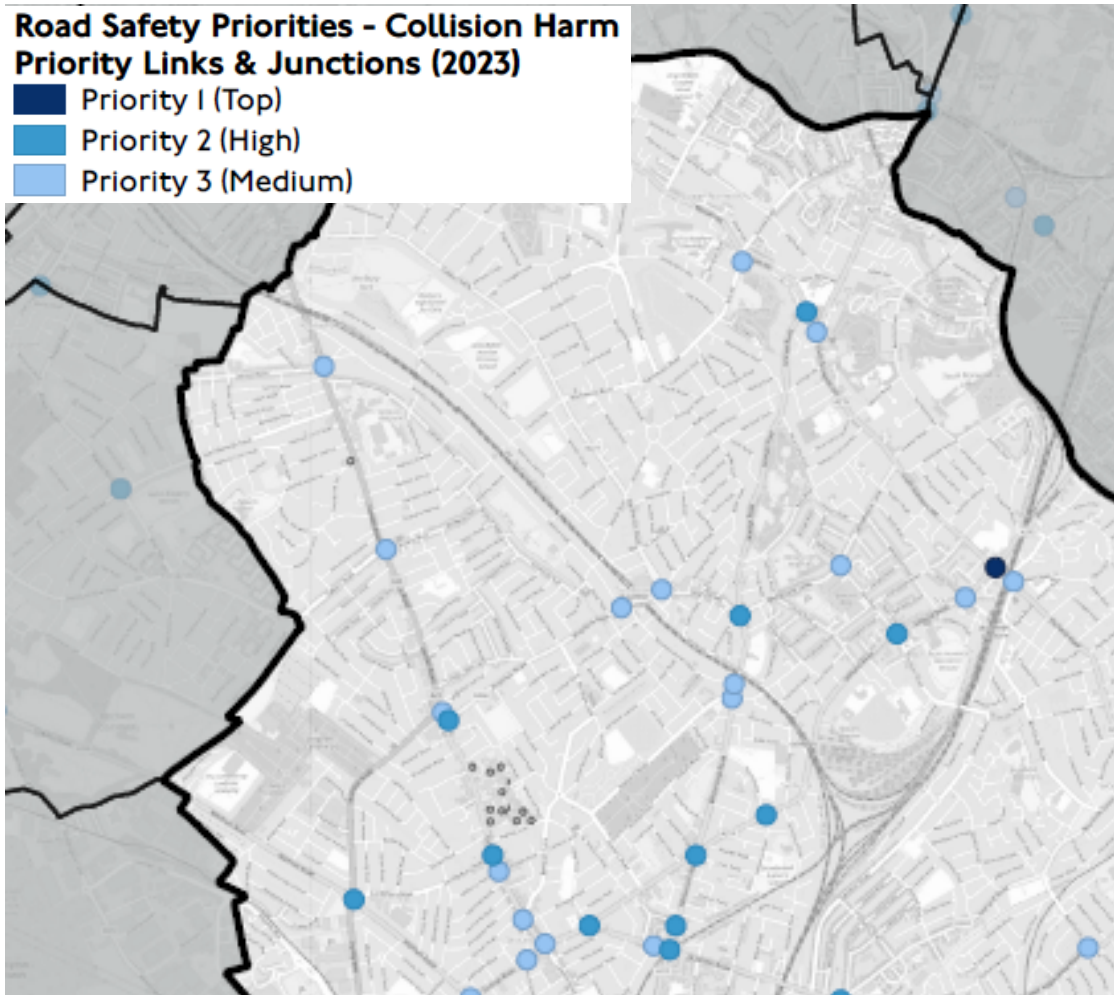


Fig 1 – Extract of road safety priority locations from TfL data pack.

- 6.5** TfL LIP funding cannot be used for routine maintenance and renewal of highway assets, although where necessary upgrades as part of infrastructure schemes can be included. In addition, TfL LIP funding cannot be used for the removal of measures previously funded via TfL, although modifications to improve schemes (for example making a Healthy School Street permanent) can be acceptable.
- 6.6** For schemes that do not meet the criteria for TfL LIP funding but have been identified as local priorities, there is the potential for them to be funded via alternative sources including Community Infrastructure Levy, Growth Zone and S106 contributions.

7 SUMMARY OF THE THREE YEAR LIP DELIVERY PLAN

- 7.1** The table below provides an overview of the proposed funding allocations over the three year plan period. Appendix B provides a summary of the overall three year plan,

with Appendix C setting out the more detailed programme for 2025/26 (which will form Croydon's annual submission).

<i>Programme</i>	<i>Funding Requested from TfL for FY 2025/26</i>	<i>Funding Requested from TfL for FY 2026/27</i>	<i>Funding Requested from TfL for FY 2027/28</i>
Town Centre Including Reconnected Croydon	£1,800,000	£1,900,000	£1,900,000
Healthy Places	£555,000	£455,000	£455,000
Walking and Cycling	£880,000	£1,200,000	£1,330,000
Safer Streets	£550,000	£550,000	£550,000
Better Buses	£1,035,000	£2,385,000	£7,885,000
Totals	£4,820,000	£6,490,000	£12,120,000

Notes:

- The increase in Walking and Cycling reflects the proposed delivery of corridor schemes from 2025/26 as part of a rolling programme based on schemes under development.
- The Safer Streets programme was not in place in 2024/25. The funding request for 2025/26 is indicative and subject to the identification of schemes in TfL priority locations.
- Better Bus Partnership funding (included in the Better Buses programme) is new for 2025/26, with the indicative funding request tapering up to reflect the identification and development of a programme in 2025/26 and then delivery of service and infrastructure improvements from 2026/27 onwards. The development of the programme will consider a range of challenges including improving north-south links, access to bus stops and improved waiting facilities, crowding and related anti-social behaviour pre and post school, as well as the density of services in the town centre.

8 ALTERNATIVE OPTIONS CONSIDERED

6.1	Do Nothing	This would prohibit the utilisation of circa £2m of external grant funding and undermine the Council's ability to deliver against identified priorities.
-----	------------	--

9 CONSULTATION

- 9.1** The LIP three Year Delivery Plan sets out how the Council will continue to deliver against the outcomes and priorities in the Croydon Local Implementation Plan 2019, which was subject to public consultation prior to publication.
- 9.2** In developing the three year Delivery Plan there has been engagement with:
- The Elected Mayor of Croydon and the Cabinet Member Streets and Environment.
 - Internal partners including Finance, Highways and Parking, Spatial Planning and Strategic Procurement.
- 9.3** It should be noted that the Council's LIP three year Delivery Plan provides an overarching framework for the development and delivery of specific infrastructure schemes. As schemes are progressed there will be both targeted engagement with

businesses and residents, representative organisations and groups as well as statutory consultation as required (for example in respect of Traffic Orders for individual schemes).

10. CONTRIBUTION TO EXECUTIVE MAYOR'S BUSINESS PLAN

10.1 The delivery of the Reconnected Croydon programme will contribute to several Council priorities and related actions:

The Council balances its books, listens to residents and delivers good sustainable services:

- Get a grip on the finances and make the Council financially sustainable.
- Ensure good governance is embedded and adopt best practice.

Action: Ensure capital projects have clear outcomes and agreed budgets that are delivered.

Croydon is a place of opportunity for business, earning and learning:

Action: Support the regeneration of Croydon's town and district centres, seeking inward investment and grants.

Lead action to reduce carbon emissions in Croydon:

Action: Develop a pipeline of retrofit projects and promote public transport and active travel.

Croydon is a cleaner, safer and healthier place, a borough we're proud to call home:

Action: Seek funding to improve the public realm of our town centre and district centres, including replacing underpasses with surface level crossings.

11. IMPLICATIONS

11.1 FINANCIAL IMPLICATIONS

11.1.1 As noted in Section 5, the indicative scale of the grant funding being provided is around £2.2m per annum. This funding is subject to annual approval by TfL so is budgeted for on an annual basis.

11.1.2 Whilst the funding is primarily intended to support the delivery of infrastructure schemes and is provided as capital, it can be utilised for revenue activities including complementary measures (such as cycle training), scheme design and development, as well as staffing costs.

11.1.3 Where opportunities have been identified for match funding (for example Growth Zone funding) this will be secured separately using established approval processes.

11.1.4 In terms of financial risks, it should be noted that failure to meet the requirements of the grant could require the Council to cover additional costs from corporate resources. This should be mitigated by ensuring that there is robust financial management and reporting.

11.1.5 It is noted that this risk must be balanced against the benefits for the Council of securing significant external funding to invest in Croydon.

Comments approved by Nish Narendran, Finance Manager on behalf of the Director of Finance on 26/09/2024.

11.2 LEGAL IMPLICATIONS

11.2.1 Section 144 of the Greater London Authority Act 1999 (GLA) places a duty on each London Local Authority to have regard to the Mayor of London's Transport Strategy when exercising any function.

11.2.2 Section 145 of the GLA requires each London Local Authority to prepare a Local Implementation Plan (LIP) containing proposals for implementing the Transport Strategy in its area.

11.2.3 The London Borough of Croydon's third LIP was approved in April 2019 and sets out the priorities proposals to support and implement the Transport Strategy.

11.2.4 Section 159 of the Greater London Authority Act 1999 allows for TfL to provide financial assistance by way of a grant, loan or other payment to provide safe, integrated, efficient and economic transport facilities or services to, from or within Greater London. This provision includes financial assistance in respect of any expenditure incurred or to be incurred by the authority in discharging any function of a highway authority or traffic authority to support the delivery of the Transport Strategy.

11.2.5 The London Borough of Croydon is recognised as a local highway authority for the purposes of the Act and is eligible to receive financial assistance from TfL to support the implementation of the Transport Strategy.

11.2.6 In exercising its powers under Section 159, TfL may have regard to any financial assistance previously given and the use made by the authority of such assistance. TfL may also impose conditions on any financial assistance it provides, including conditions for repayment in whole or in part in specified circumstances.

11.2.7 The proposed LIP 3 Year Delivery Plan has been prepared in compliance with the requirements of the GLA and in accordance with current TfL Guidance on developing borough Healthy Streets delivery plans 2025/26-27/28.

Comments approved by Kiri Bailey Head of Commercial, Housing and Litigation and Deputy Monitoring Officer on behalf of the Director of Legal Services and Monitoring Officer (3/10/2024).

11.3 EQUALITIES IMPLICATIONS

- 11.3.1 The Council has a statutory duty to comply with the provisions set out in Section 149, Equality Act 2010. The Council must therefore have due regard to:
- (a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act.
 - (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
 - (c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

The protected characteristics defined by law are race and ethnicity, disability, sex, gender reassignment, age, sexual orientation, pregnancy and maternity, religion or belief, marriage and civil partnership.

- 11.3.2 An initial EqIA assessment has been undertaken for the proposed LIP 3 Year Plan which is attached as Appendix C to this report. It considers the strategic impacts of the proposed programmes and schemes and indicates that there are some “likely to impact” and medium severity impacts for those with protected characteristics. In delivering the 3 Year Plan, further assessments will be carried out in relation to specific programmes and schemes as necessary to ensure that any equality and diversity impacts are identified and, as far as is reasonably possible, addressed to lessen any adverse impact arising from the proposed change.

Comments approved by Ken Orlukwu, Senior Equalities Officer, on behalf of Helen Reeves, Head of Strategy & Policy on 18/09/2024.

12. CRIME AND DISORDER REDUCTION IMPACT

- 12.1 Croydon Council will work with partners to ensure that the public realm and transport elements of the programme support the reduction of crime and anti-social behaviour within the borough.
- 12.2 The LIP programme fits into the Community Safety strategy priorities, in particular, “Focus on high priority neighbourhoods”.

13. DATA PROTECTION IMPLICATIONS

- 13.1 There are no data protection implications that needed to be considered. Data will be managed under the guidance of Croydon Council’s Information Management procedures and policies.

14. APPENDICES

Appendix A: TfL specified schemes, measures and locations that are eligible for LIP Safer Corridors and Neighbourhoods funding.

Appendix B: Overview of Croydon’s LIP Three Year Delivery Plan.

Appendix C: Details of Proposed Schemes for 2025/26 Annual Funding Submission.

Appendix D: LIP Three Year Plan Equality Impact Assessment (EqIA).

15. BACKGROUND DOCUMENTS

TfL ‘Guidance on developing borough Healthy Streets delivery plans 2025/26-27/28’ (August 2024).

Appendix A: TfL specified schemes, measures and locations that are eligible for LIP Safer Corridors and Neighbourhoods funding.

Schemes and measures that are eligible for LIP Safer Corridors and Neighbourhoods funding	
Core measures	Suitable locations
Traffic Reduction	
<ul style="list-style-type: none"> a) Modal filters to create area wide traffic reduction. This may include a combination of modal filters, bus gates and 'bus and cycle' only streets. b) Individual modal filters or bus gates/'bus and cycle' only streets 	<p>Any location will be considered, but the following are encouraged:</p> <ul style="list-style-type: none"> 1) Strategic Neighbourhood Analysis High need / potential locations 2) Adjoining or encompassing Strategic Cycling Analysis links 3) Encompassing or adjacent to Vision Zero Priority Links or nodes 4) Supports access to bus services
Safer Town Centres	
<ul style="list-style-type: none"> a) Modal filters, bus gates, 'bus and cycle' only streets. b) Removal of motor vehicle traffic to provide pedestrian and cycle or pedestrian only space 	<ul style="list-style-type: none"> 1) Town Centres and High Streets 2) Local and Neighbourhood Centres

Core measures	Suitable locations
School Streets	Education and learning settings including nursery, primary and secondary schools as well as higher education. The School Streets prioritisation layer can help inform need and suitability of these
Speed reduction	
20mph speed limits	Any location is supported, and boroughs are encouraged to move towards boroughwide coverage where appropriate. Schemes should be developed in line with the latest Setting Local Speed Limits guidance
Other speed limit reductions	Relevant routes
Crossings	
<ul style="list-style-type: none"> a) New zebra or parallel crossings. b) Replacement of pedestrian refuge or informal crossing with zebra or parallel crossing. c) Standalone signal controlled pedestrian / cycle crossings (either new or replacing pedestrian refuge). d) Provision of pedestrian signals on all or remaining arms of a signalised junction where none currently exist. 	<ul style="list-style-type: none"> 1) Medium to Very High Pedestrian Severance Links 2) Priority Vision Zero Links 3) Priority Vision Zero Nodes 4) Existing signalised junctions without pedestrian signals on all arms

Safer Borough Junctions	
<p>Remove conflicting movements / reduce road danger with the use of:</p> <p>a) A filter on one or more arms of the junction permitting cycling only and/or;</p> <p>b) bus gate permitting buses only on one or more arms of the junction and/or;</p> <p>c) timed filter or bus gate limiting private motor vehicle access to certain times of day;</p> <p>d) ban turning movements for general motor traffic;</p> <p>e) signal controlled pedestrian lights where they are currently missing from the arm (s) of a junction.</p>	Priority Vision Zero Nodes
Mini programmes	
<p>a) Level access for pedestrians via step-free bus stops or dropped kerbs</p> <p>b) Access barrier removal</p> <p>c) Rental micromobility parking bays</p>	Boroughwide where appropriate

Schemes and measures that are eligible for LIP Safer Corridors and Neighbourhoods funding	
Core measures	Suitable locations
<p>d) Cycle parking</p> <p>e) Cycle contraflows</p>	
Supporting Measures: these can be delivered alongside Core Measures (see Appendix 3 for suggested combinations of measures)	
<ul style="list-style-type: none"> • Sustainable drainage systems (SuDS). • Continuous footways. • Level access for pedestrians via step-free bus stops or dropped kerbs. • Footway widening • Soft landscaping such as trees or plants • Cycle contraflows • Seating • Parklets • On-street cycle and rental micromobility parking • Upgraded control access barriers • Infrastructure for cargo bikes and last mile logistics • Bus stop enhancements including accessibility upgrades • On-street or in-school cycle and children kick-scooter parking • Speed compliance measures (20mph roads only) • Wayfinding and signage including Legible London • Car club bays alongside car parking removal 	

Appendix B: Overview of Croydon's LIP Three 3 Year Delivery Plan..

PROGRAMME / Name	FY 2025/26	FY 2026/27	FY 2027/28	Overview
TOWN CENTRE INCLUDING RECONNECTED CROYDON				
Reconnected Croydon (Croydon Metropolitan Town Centre and Growth Zone Corridors)	£800,000	£900,000	£900,000	<p>Completion of Reconnected Croydon Levelling Up Fund schemes:</p> <ul style="list-style-type: none"> • London Road and West Croydon • Mid Croydon and South End • East Croydon • Fairfield • Old Town <p>Subsequent work to include Queens Square, improvements to bus operations / station interchange, waiting and loading improvements, reducing gaps in walking and cycling network, as well as public realm (including Dingwall Road North).</p>
Reconnected Croydon Brighton Road Elements	£500,000	£500,000	£500,000	<p>Ongoing corridor improvements building on existing schemes:</p> <ul style="list-style-type: none"> • Improved footways and pedestrian crossings. • Public realm improvements including tree planting and other greening. • Additional parking, particularly in relation to local businesses. • Review of cycle lanes focusing on highway safety.
Reconnected Croydon London Road Elements	£500,000	£500,000	£500,000	<p>Ongoing corridor improvements building on existing schemes:</p> <ul style="list-style-type: none"> • Improved footways and pedestrian crossings. • Public realm improvements including tree planting and other greening. • Additional parking, particularly in relation to local businesses.

PROGRAMME / Name	FY 2025/26	FY 2026/27	FY 2027/28	Overview
				<ul style="list-style-type: none"> Review of cycle lanes focusing on highway safety.
	£1,800,000	£1,900,000	£1,900,000	
HEALTHY PLACES				
Addiscombe East - West Neighbourhood	£435,000	£205,000	£0	<p>Delivery of wide area healthy streets scheme following completion of co-design process with local community.</p> <p>Measures likely to include:</p> <ul style="list-style-type: none"> Improved footways and pedestrian crossings. Public realm improvements including tree planting and other greening. Point closures to reduce traffic flows in local streets. Monitoring and modification.
Existing Healthy Neighbourhoods Adjustments	£20,000	£50,000	£255,000	Development and delivery of potential extension of existing healthy streets following engagement with local community.
Rolling programme of delivery of Healthy School Streets	£100,000	£200,000	£200,000	<p>Identification, engagement, design and delivery of further healthy school streets which combine measures to improve road safety (including reducing vehicle speeds and volumes) and support walking and cycling.</p> <p>Includes provision for complementary measures to support schools, for example cycle parking.</p>
	£555,000	£455,000	£455,000	
WALKING AND CYCLING				
Mitcham Common - Wandle Park	£345,000	£400,000	£400,000	Design and delivery of further phases of a long-term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.
Lloyd Park - Addington Park	£355,000	£400,000	£400,000	Design and delivery of further phases of a long-term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.

PROGRAMME / Name	FY 2025/26	FY 2026/27	FY 2027/28	Overview
Lloyd Park - South Norwood Country Park	£50,000	£400,000	£400,000	Design and delivery of further phases of a long-term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.
Cycle Parking	£54,000	£54,000	£54,000	Delivery of circa 10 cycle hangars per annum to reduce cycles being stored in in appropriate locations.
Cycle Training	£76,000	£76,000	£76,000	Delivery of cycle training for children and adults to increase confidence and improve road safety.
	£880,000	£1,330,000	£1,330,000	
SAFER STREETS				
Lower Addiscombe Road	£50,000	£500,000		Design and then delivery of road safety measures, such as new crossings and junction improvements, in priority locations.
Brigstock Road		£50,000	£500,000	Design and then delivery of road safety measures, such as new crossings and junction improvements, in priority locations.
Portland Road	£500,000			Design and then delivery of road safety measures, such as new crossings and junction improvements, in priority locations.
Crown Point			£50,000	Design and then delivery of road safety measures, such as new crossings and junction improvements, in priority locations.
	£550,000	£550,000	£500,000	
BETTER BUSES				
Better Bus Partnership	£500,000	£2,000,000	£7,500,000	Review of bus operations (including stops, stands and routeing) in Croydon Town Centre which aims to make them more efficient. Initial area of investigation will be alignment of SuperLoop services to improve interchange. Funding in future years will be for delivering changes.
Bus Priority: Kent Gateway	£300,000			Delivery of improvements at Addington Village interchange to reduce bus journey time delays and improve access.

PROGRAMME / Name	FY 2025/26	FY 2026/27	FY 2027/28	Overview
Bus Priority	£235,000	£385,000	£385,000	Design and delivery of a rolling programme of small-scale interventions to address local pinch points and improve access to services.
	£1,035,000	£2,385,000	£7,885,000	

Appendix C: Details of Proposed Schemes for 2025/26 Annual Funding Submission.

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
TOWN CENTRE INCLUDING RECONNECTED CROYDON					
Reconnected Croydon (Croydon Metropolitan Town Centre and Growth Zone Corridors)	£800,000	<p>Completion of Reconnected Croydon Levelling Up Fund schemes:</p> <ul style="list-style-type: none"> • London Road and West Croydon • Mid Croydon and South End • East Croydon • Fairfield • Old Town <p>Post Levelling Up Fund work to include Queens Square, improvements to bus operations / station interchange, waiting and loading improvements, reducing gaps in walking and</p>	<p>Proposals identified in Croydon Opportunity Area Planning Framework, Infrastructure Delivery Plan, and Growth Zone Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have been implemented in the LIP using TfL and other funding.</p> <p>The funding allocation will be used as part of the match funding package to support the delivery of the Reconnected</p>	<p>The scheme is located in the Croydon Metropolitan Centre Opportunity Area and along key corridors into the centre.</p> <p>The interventions are highways and public realm schemes as set out in the Reconnected Croydon Levelling Up Fund submission, which are primarily in the Croydon Opportunity Area.</p> <p>Potential improvements identified outside</p>	<p>As per the original Levelling Up Fund submission a range of schemes have been identified to deliver improvements including:</p> <ul style="list-style-type: none"> •Highway safety interventions. Core measures include traffic reduction to provide pedestrian and cycle as part of safer town centres. •Public realm improvements including greening. •Removal of subways and provision of at grade crossings. • Improvements cycle lanes focusing on highway safety.

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
		cycling network, as well as public realm (including Dingwall Road North).	Croydon programme.	of the core scope will be picked up in future programmes for LIP and / or other funding sources.	<ul style="list-style-type: none"> ●Wayfinding and signage. ●Bus priority and accessibility measures
Reconnected Croydon Brighton Road Elements	£500,000	<p>Ongoing corridor improvements building on existing schemes:</p> <ul style="list-style-type: none"> • Improved footways and pedestrian crossings. • Public realm improvements including tree planting and other greening. • Additional parking, particularly in relation to local businesses. • Improvements to cycle lanes focusing on highway safety. 	<p>Proposals identified in Croydon Opportunity Area Planning Framework, Infrastructure Delivery Plan, and Growth Zone Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have been implemented in the LIP using TfL and other funding.</p> <p>As part of the Growth Zone Croydon has identified four corridors within the Borough where walking, cycling and</p>	<p>The scheme is located in the Croydon Metropolitan Centre Opportunity Area and along key corridors into the centre.</p> <p>The interventions are highways and public realm schemes as set out in the Reconnected Croydon Levelling Up Fund submission, which are primarily in the Croydon Opportunity Area (High St -South End -Whitgift).</p> <p>In addition safety</p>	<p>As per the original Levelling Up Fund submission a range of schemes have been identified to deliver improvements including:</p> <ul style="list-style-type: none"> ●Highway safety interventions. Core measures include traffic reduction to provide pedestrian and cycle as part of safer town centres, local, and neighbourhood centres ●Improved footways and pedestrian crossings. ●Public realm improvements including tree planting and other greening. ● Improvements to

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
			<p>public transport will be encouraged by creating safer and more attractive environments for these modes of transport.</p> <p>The funding allocation will be used as part of the match funding package to support the delivery of the Reconnected Croydon programme.</p>	<p>improvements are proposed to resolve safety concerns at the at the Riddlesdown Road / Brighton Road junction.</p> <p>Potential improvements identified outside of the core scope will be picked up in future programmes for LIP and / or other funding sources.</p>	<p>cycle lanes focusing on highway safety.</p> <ul style="list-style-type: none"> •Bus priority and accessibility measures
Reconnected Croydon London Road Elements	£500,000	<p>Ongoing corridor improvements building on existing schemes:</p> <ul style="list-style-type: none"> • Improved footways and pedestrian crossings. • Public realm improvements including tree planting and other greening. • Additional 	<p>Proposals identified in Croydon Opportunity Area Planning Framework, Infrastructure Delivery Plan, and Growth Zone Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have</p>	<p>The scheme is located in the Croydon Metropolitan Centre Opportunity Area and along key corridors into the centre.</p> <p>The interventions are highways and public realm schemes as set out in the</p>	<p>Ongoing corridor improvements building on existing schemes:</p> <ul style="list-style-type: none"> •Highway safety interventions. Core measures include traffic reduction to provide pedestrian and cycle as part of safer town centres, local, and neighbourhood centres

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
		<p>parking, particularly in relation to local businesses.</p> <ul style="list-style-type: none"> • Improvements to cycle lanes focusing on highway safety. 	<p>been implemented in the LIP using TfL and other funding.</p> <p>As part of the Growth Zone Croydon has identified four corridors within the Borough where walking, cycling and public transport will be encouraged by creating safer and more attractive environments for these modes of transport</p> <p>The funding allocation will be used as part of the match funding package to support the delivery of the Reconnected Croydon programme.</p>	<p>Reconnected Croydon Levelling Up Fund submission, which are primarily in the Croydon Opportunity Area.</p> <p>Potential improvements identified outside of the core scope will be picked up in future programmes for LIP and / or other funding sources.</p>	<ul style="list-style-type: none"> •Improved footways and pedestrian crossings. •Public realm improvements including tree planting and other greening. •Exploratory work to identify parking bays on main roads, subject to future changes • Improvements to cycle lanes focusing on highway safety. •Bus priority and accessibility measures
	£1,800,000				
HEALTHY PLACES					

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
Addiscombe East - West Neighbourhood	£435,000	<p>Delivery of wide area healthy streets scheme following completion of co-design process with local community.</p> <p>Measures likely to include:</p> <ul style="list-style-type: none"> • Improved footways and pedestrian crossings. • Public realm improvements including tree planting and other greening. • Point closures to reduce traffic flows in local streets. • Monitoring and modification. 	Identified in the Third Croydon Local Implementation Plan (2019). Previous phases of the scheme have been implemented using TfL and other funding.	<p>Addiscombe East / West Wards. Locations based on:</p> <ol style="list-style-type: none"> 1. Strategic Neighbourhood Analysis High need / potential locations 2. Adjoining or encompassing Strategic Cycling Analysis links 3. Encompassing or adjacent to Vision Zero Priority Links or nodes 4. Supports access to bus services 	<p>Delivery of wide area improvements following completion of co-design process with local community. Measures likely to include:</p> <ul style="list-style-type: none"> •Traffic Reduction in specific residential streets to include access restrictions to create quieter and pleasant streets for both pedestrians and cyclists •New pedestrian crossings and side road junction improvements • Zebra crossings • Signal controlled pedestrian/ cycle crossings • Sustainable drainage systems (SuDS). • Continuous footways. • Level access for pedestrians via step-free bus stops or

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
					<p>dropped kerbs.</p> <ul style="list-style-type: none"> • Footway widening • Soft landscaping such as trees or plants • Cycle contraflows • Seating • On-street cycle parking
Existing Healthy Neighbourhoods Adjustments	£20,000	Development and delivery of potential extension of existing healthy streets following engagement with local community.	Identified in the Third Croydon Local Implementation Plan (2019). Previous phases of the scheme have been implemented using TfL and other funding.	<p>Broad Green, West Thornton, South Norwood, Woodside, Fairfield, and Addiscombe West wards. Locations based on:</p> <ol style="list-style-type: none"> 1. Strategic Neighbourhood Analysis High need / potential locations 2. Adjoining or encompassing Strategic Cycling Analysis links 3. Encompassing or adjacent to Vision Zero Priority Links or nodes 	<p>Adjustments to existing healthy neighbourhoods to improve conditions in Wentworth and priority roads adjacent to the Sutherland Healthy Neighbourhood</p> <p>Core measures Traffic Reduction</p> <p>a) potential adjustments to two roads which may include ANPR access restrictions or one way systems to stem traffic flow to lessen congestion and improve conditions for both pedestrians and</p>

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
				4. Supports access to bus services	<p>cyclists</p> <p>Measures likely to include:</p> <ul style="list-style-type: none"> · Improved footways and pedestrian crossings. · Public realm improvements including tree planting and other greening. · Point closures to reduce traffic flows in local streets. · Monitoring and modification.
Rolling programme of delivery of Healthy School Streets	£100,000	Identification, engagement, design and delivery of further healthy school streets which combine measures to improve road safety (including reducing vehicle speeds and volumes) and support walking and cycling.	Identified in the Third Croydon Local Implementation Plan (2019). Previous phases of the scheme have been implemented using TfL and other funding.	Boroughwide. Use of data-driven approaches to identify high-priority schools. Criteria for selecting appropriate location includes locations with: <ul style="list-style-type: none"> - High traffic volumes. - Significant road safety concerns. - Poor air quality. 	<p>Identification, engagement, design and delivery of further healthy school streets which combine measures to improve road safety (including reducing vehicle speeds and volumes) and support walking and cycling.</p> <p>Includes provision for complementary measures to support</p>

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
		Includes provision for complementary measures to support schools, for example cycle parking.		- Low active travel rates.	schools, for example cycle parking, safer crossings, footway / cycle path improvements.
	£555,000				
WALKING AND CYCLING					
Mitcham Common - Wandle Park	£345,000	Design and delivery of further phases of a long-term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.	<p>Identified in 'Green Grid' Croydon's Local Plan Policies and supporting Infrastructure Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have been implemented using TfL LIP and other funding.</p> <p>Detailed design is being progressed on phases to be delivered in Q4 2024/25 onwards (Ampere Way</p>	<p>West Thornton, Broad Green, and Waddon.</p> <p>Routes chosen based on:</p> <ol style="list-style-type: none"> 1) Alignment with priority connections identified in the strategic cycling analysis 2022 (SCA) 2) Intervention that fill a gap in the existing Cycleway network 3) Providing additional local connectivity to the Cycleway network 4) Links or nodes 	<p>Measures likely to include:</p> <p>New Cycleway scheme meeting the New Cycleway Quality Criteria (CQC).</p> <p>Cycleway schemes to include:</p> <ol style="list-style-type: none"> a) Protected routes on busier roads (as per CQC) b) New cycle crossings (parallel or toucan crossings) c) Upgrade of informal crossings to parallel or toucan crossings

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
			crossing and links to Valley Park and along tram route. Submission to TfL for funding this year to part build the scheme and complete in 2025-26 (subject to the award of this additional TfL funding).	on existing Cycleway routes that do not meet Cycle Quality Criteria standards	
Lloyd Park - Addington Park	£355,000	Design and delivery of further phases of a long term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.	<p>Identified in 'Green Grid' Croydon's Local Plan Policies and supporting Infrastructure Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have been implemented using TfL LIP and other funding.</p> <p>Detailed design is being progressed on phases to be delivered in Q4</p>	<p>Park Hill & Whitgift, South Croydon, and Selsdon & Addington Vale. Routes chosen based on:</p> <ol style="list-style-type: none"> 1) Alignment with priority connections identified in the strategic cycling analysis 2022 (SCA) 2) Intervention that fill a gap in the existing Cycleway network 3) Providing 	<p>Measures likely to include:</p> <p>New Cycleway scheme meeting the New Cycleway Quality Criteria (CQC). Cycleway schemes to include:</p> <ol style="list-style-type: none"> a) Protected routes on busier roads (as per CQC) b) New cycle crossings (parallel or toucan crossings) c) Upgrade of informal crossings to parallel or toucan crossings

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
			<p>2024/25 onwards (link from existing cycle park to Coombe Wood School and further along Coombe Road. Submission to TfL for funding this year to part build the scheme and complete in 2025-26 (subject to the award of this additional TfL funding).</p>	<p>additional local connectivity to the Cycleway network 4) Links or nodes on existing Cycleway routes that do not meet Cycle Quality Criteria standards</p>	
Lloyd Park - South Norwood Country Park	£50,000	<p>Design and delivery of further phases of a long term corridor scheme which improves road safety for people walking and cycling by providing segregated routes between key destinations.</p>	<p>Identified in 'Green Grid' Croydon's Local Plan Policies and supporting Infrastructure Delivery Plan. A long term and phased scheme.</p> <p>Previous phases of the scheme have been implemented in the LIP using TfL and other funding.</p> <p>A long term and</p>	<p>Park Hill & Whitgift, Addiscombe East & West, and Woodside. Routes chosen based on: 1) Alignment with priority connections identified in the strategic cycling analysis 2022 (SCA) 2) Intervention that fill a gap in the</p>	<p>Measures likely to include: New Cycleway scheme meeting the Measures likely to include: New Cycleway Quality Criteria (CQC). Cycleway schemes to include: b) Protected routes on busier roads (as per CQC) c) New cycle crossings (parallel or</p>

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
			<p>phased scheme which improves road safety for people walking and cycling by providing a mixture of segregated / low traffic / traffic free routes between key destinations. Linked to the Existing Healthy Neighbourhood schemes (see above).</p>	<p>existing Cycleway network 3) Providing additional local connectivity to the Cycleway network 4) Links or nodes on existing Cycleway routes that do not meet Cycle Quality Criteria standards</p>	<p>toucan crossings) d) Upgrade of informal crossings to parallel or toucan crossings e) Contraflows</p> <p>Scheme provides links via modal filters of Group 1 Healthy Neighbourhood Projects (see above).</p>
Cycle Parking	£54,000	<p>Delivery of cycle training for children and adults to increase confidence and improve road safety.</p>	<p>Identified in the Third Croydon Local Implementation Plan (2019). Previous phases of the scheme have been implemented using TfL and other funding.</p>	<p>Boroughwide. Boroughs are being provided with an equal base allocation of funding for cycle training on an annual basis, with a further activation fund linked to infrastructure delivery and cycle potential, as agreed through the LIP working group.</p>	<p>Delivery of cycle training for children and adults to increase confidence and improve road safety.</p>

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
Cycle Training	£76,000	Delivery of circa 10 cycle hangars per annum to reduce cycles being stored in in appropriate locations.	Identified in the Third Croydon Local Implementation Plan (2019). Previous phases of the scheme have been implemented using TfL and other funding.	Boroughwide. Suitable locations identified from the following: 1) Council housing/ housing association land 2) Areas of higher deprivation (Index of Multiple Deprivation between 1 to 4) 3) In an area-wide traffic reduction scheme 4) Connected to the strategic cycle network i.e., within 400m of the signed Cycleway network, including former Quietways/Cycle Superhighways, or within 400m of a Strategic Cycling Analysis (SCA) connector	Delivery of circa 10 cycle hangars per annum to reduce cycles being stored in in appropriate locations.
	£880,000				
SAFER STREETS					

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
Lower Addiscombe Road	£50,000	Design of road safety interventions in priority location.	New funding area. Initial work has been done by Croydon to identify collision clusters and create design concepts for improvements at these locations.	Addiscombe East & West. Locations chosen based on collision data analysis and proximity to other schemes.	Measures likely to include: Design and then delivery of road safety measures, such as new crossings and junction improvements, in priority locations. Supporting measures can include sustainable urban drainage, soft landscaping and level access for pedestrians.
Portland Road	£500,000	Design and delivery of road safety interventions in priority location.	New funding area. Initial work has been done by Croydon to identify collision clusters and create design concepts for improvements at these locations.	Location chosen based on collision data analysis and proximity to other schemes.	Measures likely to include: Design and then delivery of road safety measures, such as crossing and junction improvements, in priority location. Supporting measures could include sustainable urban drainage, soft landscaping and level access for pedestrians.

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
	£550,000				
BETTER BUSES					
Better Bus Partnership	£500,000	Development of proposals to improve bus operations and services in and around Croydon.	New funding area. Initial work has been done by Croydon to identify junctions for improved bus priority and create design concepts for improvements at these locations.	Croydon Metropolitan Centre Opportunity Area.	Improvement of bus operations (including stops, stands and routing) in Croydon Town Centre which aims to make them more efficient. Initial area of investigation will be alignment of SuperLoop services to improve interchange. Funding in future years will be for delivering changes.
Bus Priority: Kent Gateway	£300,000	Signals and access improvements at Addington Village bus interchange.	Ongoing delivery of project in the 2024/25 Local Implementation Plan.	Selsdon & Addington Vale. Alignment with bus underperformance identified in the TfL's Strategic Bus Analysis.	Continuing Delivery of improvements at Addington Village interchange to reduce bus journey time delays and improve access.
Bus Priority	£235,000	Rolling programme of delivery of small-scale interventions to improve bus progression	Ongoing programme focused on pinch points and junctions that are identified as having	Boroughwide.	Smaller interventions that improve bus journey times and reliability. Examples include bus lane and

PROGRAMME / Scheme Name	Funding Requested for FY 2025/26	Overview of Scheme	Context	Scope	Potential Measures
			a negative impact on bus operations.		parking control extensions.
	£1,035,000		n Plan.	s.	