

PLANNING COMMITTEE AGENDA

PART 6: Planning Applications for Decision

Item 6.1

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 18/05858/FUL
Location: 6A The Drive, Coulsdon, CR5 2BL
Ward: Coulsdon Town
Description: Demolition of the existing dwelling and erection of a part three part four storey development for nine apartments with associated access, six off-street parking spaces, cycle storage and refuse store.
Drawing Nos: PL_005, PL_99_02, PL_99_02, PL_100_02, PL_101_02, PL_102_02, PL_103_02, PL_50_02, PL_200_03, PL_300_03, PL_202_03, PL_203_03, Amended Tree Report, Amended Tree Schedule, Amended Tree Constraints Plans, Topographical Survey, Design and Access Statement, Day and Sunlight Report, Flood Risk Assessment and Parking Stress Survey.
Applicant: Macar Developments
Agent: Mr Paul Lewis
Case Officer: Henrietta Ansah

	studio	1 bed	2 bed	3 bed	4 bed
Existing houses					1
Proposed Flats		2	5	2	

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
6	18

- 1.1 This application is being reported to committee because the ward councillor Luke Clancy has made representations in accordance with the Committee Consideration Criteria and requested committee consideration and representations, including a petition, above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

- 2.1 That the Planning Committee resolve to GRANT planning permission
- 2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Development to be carried out in accordance with the approved drawings
2. Materials to be submitted

3. Details of Refuse/Cycles/Boundary/EVCP
4. Details of hard and soft landscaping
5. Construction Logistics Plan to be submitted
6. Car parking provided as specified
7. No additional windows in the flank elevations
8. 19% reduction in carbon emission over the 2013 Building Regulations
9. Water consumption target of 110 litres per head per day
10. Permeable forecourt material to be used
11. Trees - Accordance with the Arboricultural Report
12. Tree - Protection for street trees and trees at rear
13. Inclusive access ground floor
14. Visibility Splays
15. In accordance with details of FRA
16. Time limit of 3 years
17. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practise for Construction Sites
- 3) Wildlife protection
- 4) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.3 That the Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing four bedroom detached house
- Erection of a two storey building with accommodation in roofspace
- Provision of 9 flats
- Provision of 6 off-street car parking spaces
- Provision associated refuse/cycle stores at the front and rear of the site
- Reduction in ground level

3.2 As part of the application the applicant has submitted amended plans. None of the amendments require a re-consultation. The changes are detailed below:

- Lowered front wall adjacent to ground floor flats
- The incorporation of landscaping

Site and Surroundings

3.3 The application site is situated on the north-western side of The Drive and currently comprises a part 1- part 2-storey dwellinghouse with a side addition and a detached garage at the site frontage.

- 3.4 The site slopes steeply up from the street frontage, therefore the existing house is substantially elevated above street level and is accessible via a footpath and steps.
- 3.5 To the rear of the site the property has been extended to the side, a conservatory is located to the rear of the existing property and a large elongated dormer addition is located on the rear roofslope. The rear garden is extensive and is laid to lawn.
- 3.6 The north-western side of The Drive is at a significantly higher level than properties on the opposite side.
- 3.7 The site is located in a mainly residential area comprising many detached properties of differing designs, heights, massing, and eras.
- 3.8 The site lies within a critical drainage surface water area. There are no designations attached to the site. The site has a public transport accessibility level of 3.



Figure 1: Aerial street view highlighting the proposed site within the surrounding streetscene

Planning History

3.9 There is no previous planning history for the site.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable given the residential character of the surrounding area.
- The design and appearance of the development is appropriate
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.

- The living standards of future occupiers are satisfactory and Nationally Described Space Standard (NDSS) compliant.
- The level of parking and impact upon highway safety and efficiency is considered acceptable and can be controlled through conditions.
- The impact on trees and proposed landscaping is satisfactory and can be controlled by conditions.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

- 5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

- 6.1 The application has been publicised by 11 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received from neighbours, local groups

No of individual responses: Objecting:32 Supporting: 0 Comment:0

- 6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objections

Summary of objections	Response
<i>Family Housing</i>	
<ul style="list-style-type: none"> • Loss of family housing replacement with flats 	The scheme would not result in the net loss of three-bedroom accommodation with as the original property has 4 bedrooms. Nevertheless family housing is proposed by way of 2 x 3 bed units re-provided alongside 4 x 2 bed (4-person units). The scheme would provide a high percentage of family accommodation and would meet the need for such provision.
<i>Residential Amenity Considerations</i>	
Impact on residential amenity of adjoining occupiers – loss of privacy and light, noise and disturbance.	In view of the separation distances afforded and rear projections, officers are satisfied that the scheme would not lead to an acceptable loss of amenity to the adjoining occupiers. Privacy would be protected through use of obscure glazing and high level windows.
<i>Access and Parking</i>	
Traffic congestion/Impact on highway safety	Whilst the site has a moderate PTAL level (PTAL 3) the site is relatively close

Inadequate parking provision	to bus and train services and on-site car parking is proposed in the form of 0.66 spaces per unit car parking. Cars would be able to manoeuvre on site and exit safely in forward gear.
<i>Design and Scale of Development/Overdevelopment of the site</i>	
<ul style="list-style-type: none"> • Character of the area and design Flats are not in keeping with the area Overdevelopment Bulky	<p>The current bungalow represents an under-utilisation of this relatively substantial site and there is variety of built forms in the area.</p> <p>The site is considered to be of a sufficient size to accommodate the quantum of development proposed and the building sits comfortably in its context, providing acceptable levels of parking, internal layouts and amenity space in accordance with policy requirements. It is not considered the proposal would constitute overdevelopment.</p>
<i>Environmental Considerations</i>	
Noise, dust and disturbance from construction works	This will be controlled and managed through a construction logistics/management plan.
Flood risk	The site lies within an area of surface water flooding and the applicant has put forward mitigation measures.
Waste	Sufficient waste storage have been provided, in any case this can be controlled by condition.

6.3 The following procedural or non-material issues were raised in representations and are addressed below:

- Development would lead to more tenants than owners. The road suffered before from having tenants especially with loud music, crowded parking and burglaries problems
- Downgrading of housing stock
- Depreciation of market stock
- Risk to foundations
- Subsidence

6.4 An objection has been raised by Purley and Woodcote Residents Association:

- Unsympathetic
- Higher and bulkier than neighbours
- Loss of amenity
- Lack of consultation by the applicant

- Inappropriate development

6.5 Cllr Luke Clancy objecting:

- Overdevelopment
- Out of keeping with the area
- Parking

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in July 2018. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture

- 7.21 Woodlands and trees

7.5 Croydon Local Plan 2018

- SP2 - Homes
- SP6.3 - Sustainable Design and Construction
- DM1 - Housing choice for sustainable communities
- DM10 - Design and character
- DM13 - Refuse and recycling
- DM18 - Heritage assets and conservation
- DM23 - Development and construction
- DM28 - Trees
- DM29 - Promoting sustainable travel and reducing congestion
- DM30 - Car and cycle parking in new development

7.6 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required are as follows:

1. Principle of development
2. Townscape and visual impact
3. Housing quality for future occupiers
4. Residential amenity for neighbours
5. Access and parking
6. Sustainability and environment
7. Trees and landscaping
8. Other matters

Principle of Development

- 8.2 The Croydon Local Plan 2018 identifies that a third of housing should come from windfall sites and suburban intensification, in order to protect areas such as Metropolitan Green Belt. Coulsdon has been identified as an area of sustainable growth of the suburbs for accommodating medium and moderate residential growth.
- 8.3 There is a requirement that there should not be a net loss of 3 bedroom houses or the loss of homes smaller than 130m². The applicant site currently accommodates a 4 bedroom property with a GIA of 179m², and thus this policy requirement would not apply.
- 8.4 The Council has a strategic target for 30% of all new homes up to 2036 to have three or more bedrooms. The proposed development includes 2 x three bedroom properties and 2 x two bedroom 4 person properties, which would in total provide 44% family accommodation, in excess of minimum requirements.
- 8.5 In respect to the density of the scheme representations have raised concern over the overdevelopment. In respect to the density of the scheme, representations have

raised concern over the intensification of the site and overdevelopment. The site is a suburban setting with a PTAL rating of 3 and as such, the London Plan indicates that the density levels ranges of 150–250 hr/ha habitable rooms per hectare (hr/ha); the proposal would be in excess of this range (352 hr/ha). However, the London Plan density matrix is a guide and cannot be used as a prescriptive measure. Furthermore, the London Plan provides sufficient flexibility for higher density schemes to be supported. In this instance, the proposal, as set out below, is considered to sit comfortably in the streetscene and not have a detrimental impact on neighbouring properties and to be overall a successful development, so this density is considered to be acceptable.

- 8.6 Given the residential nature of the surrounding area, the principle of the redevelopment for a residential property can be supported. The development would provide additional residential units including family homes in an established residential area. There is no in principle objection to the proposal.

Townscape and Visual Impact

- 8.7 The application site is a generous plot of significant depth within an established residential area, which is capable of accommodating additional units to maximise its use. At present the property is significantly smaller than the neighbouring and surrounding properties in terms of its massing and height.
- 8.9 Policy DM10.1 states that proposals should achieve a minimum height of 3 storeys, and the proposal is for a part four/part three storey building with accommodation in the roofspace. A lower ground floor has been created to utilise the topography of the land to provide residential accommodation. The lower ground floor would be set behind screening and set back from the highway, and thus would not appear prominent in the streetscene. In addition, the ridge height is only marginally higher than the surrounding properties. This approach is considered to make the best use of the site and sit comfortably with the surrounding area, subject to satisfactory screening of the lower ground floor and hardstanding area.
- 8.10 The proposed building would be in keeping with the surrounding area by way of the prominent gable roof profile, and detailing. The overall height would not be dissimilar to the neighbouring properties. The proposed recessed central glazed core, projecting front gables, windows and iron balustrades add an element of variation, contrast and contemporary design to the proposed form. The rear projection maximises the sites potential whilst respecting the overall pattern and layout of development in the area.



Figure 2: Front elevation



Figure 3: Rear elevation

- 8.11 The facades will be built using Kassandra red sand faced brick and smokey grey mud creased brick. Windows will grey upvc and full height doors grey aluminium. The roof will be in slate coloured tiles. Balustrade will be grey to match the windows. There will also be laser cut feature panels to the front and rear elevations in matching grey. Good quality brick in 2 tones has been proposed alongside deep reveals, especially to the bay windows, to provide the appearance of robust longevity and provide good shadow lines. Details of all the materials will be suitably conditioned. Overall, the development would comply with policy objectives in terms of local character and would respect the form, urban grain and architectural integrity of the surrounding properties.



Figure 4: A perspective image of proposed development within the streetscene

Housing Quality for Future Occupiers

- 8.12 Each of the units within the proposal would comply with internal dimensions and minimum GIA required by the Nationally Described Space Standards, in terms of the overall floor area and the size of individual rooms. In terms of layout the proposed units are considered acceptable to the amenities of any future occupiers providing high quality living accommodation which is significantly above the minimum housing standards.

- 8.13 All of the units would be dual aspect and have suitable ventilation and natural day and sunlight. The habitable rooms all face to the rear and front of the site providing a high standard of outlook. A daylight and sunlight report has been undertaken which demonstrates that each of the units would provide adequate daylight and sunlight in exceeding the BRE minimum recommended levels.

- 8.14 The lower ground floor units benefit from front facing window and additional high level obscurely glazed windows to the side. Again, the daylight and sunlight report demonstrate that these units would receive adequate levels of daylight and sunlight.

- 8.15 The London Housing Supplementary Planning Guidance (SPG) sets out that a minimum of 5 square metres of *external* amenity space be provided for a 1-2 person dwelling, with 1 additional square metres for every additional occupant. Thus the 1 bedroom units would be expected to provide 5 sqm of amenity space; 2 bedroom units would be expected to provide 7-8 sqm of amenity space; and the 3 bedroom units would be expected to provide 9 sqm of amenity space. All of the private amenity areas will meet or exceed minimum requirements.

- 8.16 6 of the 9 units would have directly attached private amenity space areas, by way of terraces and balconies. 2 of the 9 units would have private amenity spaces in a level secluded part of the rear garden within private amenity pods, finished in Indian Sandstone, and surrounded by medium sized shrubs. On balance the solution to providing amenity for this scheme is the best solution given the constraints and alternative options already explored.

- 8.17 These allocated private amenity ‘pods’ will serve the smaller units, all of the family accommodation will have directly attached and accessible amenity space, in excess. Moreover, the three bedroom units would be located on the ground floor with access to a terrace area measuring 36.5 sqm in excess of minimum standards.
- 8.18 The lower ground floor units would have amenity space to the front, which would be separated from the car parking area and adequately screened by a low boundary wall and hedging which would provide a privacy and acoustic barrier. These areas will provide good amenity space whilst ensuring the front would be well maintained in the future. In addition, high quality communal amenity space provision of approximately 30 sqm is provided in the rear garden. 15.8 sqm of play space has also been provided within the communal garden area, which would satisfy policy requirements.
- 8.19 A through route will be provided to allow direct access into the rear communal amenity space.
- 8.20 The proposed units are 100% M4(2) (accessible and adaptable dwellings) compliant in terms of their internal layouts. However given the extensive gradient at the front of the property and the slope as you ascend The Drive, M4(3) (wheelchair user dwellings) compliance is not achievable, without the addition of intrusive retaining walls and ramps. The Mayors Housing SPG permits an element of flexibility on M4(3) and M4(2) provision on sites of four stories or less to ensure that sites are deliverable. Taking into account the site constraints, on balance that this is considered acceptable.
- 8.21 The development is considered to result in a high quality development including a provision of 44% family units all with adequate amenities and provides a good standard of accommodation for future occupiers.

Residential Amenity for Neighbours

- 8.22 The properties that have the potential to be most affected are the adjoining properties No 8 and No. 6 The Drive. The proposed development would be set away from the side boundary of No, 6 by 1.6 metres; and from No. 8 by 1 metre. The flank to flank wall separation distances would be 7.25 metres to No 6, and 5.25 metres to No. 8.
- 8.23 No 6 has a window in the side of the existing gable end. The applicant has undertaken a layout appraisal and this window serves a bedroom attic. No. 8 has bedroom window also within the side gable at roof level, facing the application site. A 25-degree BRE guidance test has been undertaken to establish whether these windows would be affected by the proposed development. Both of these windows are outside of the 25-degree line taken from the neighbouring properties (shown below), due to the design of the barn hipped roof profile of the proposed development, and thus would not be unduly affected, by a loss of daylight or sunlight.



Figure 5: Rear elevation showing the 25-degree line from neighbouring flank wall windows.

- 8.24 The existing property extends beyond the rear building line of the adjoining occupiers by at least 2 metres. The main bulk of the proposed development would extend 4-4.5 metres beyond the rear building lines of No. 6 and No. 8. Beyond which is a centralised projecting element. The 45-degree line of sight from No. 6 and No. 8's nearest rear facing habitable room windows would not be breached, and thus these habitable rooms would not be unduly affected by the proposed development.

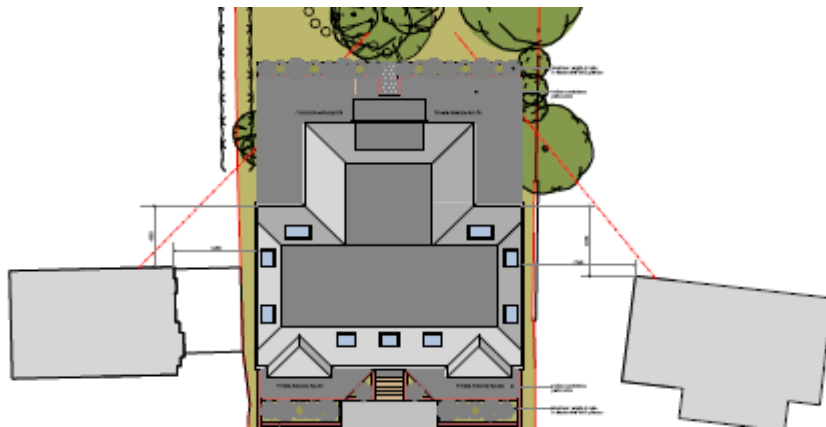


Figure 6: proposed massing showing the 45-degree line of sight at the rear

- 8.26 The windows proposed on the lower ground and ground floor side elevations will be obscurely glazed and high level to prevent any loss of privacy. At first floor level, secondary side facing windows serve living room areas and would include louvered timber panels, allowing light through but preventing any overlooking.
- 8.27 The balcony and terrace areas will be well screened or inset and would not cause any overlooking. Screening will be duly conditioned.
- 8.28 The proposed rooflights are positioned at roof level and would not be directly facing. They would provide adequate light without any direct overlooking. Any views afforded would be oblique.
- 8.29 Given the design, layout and separation between the properties, boundary treatment and provision of a suitable landscaping scheme (secured by way of a

planning condition) this is deemed acceptable to ensure no undue impact on the amenities of neighbouring properties.

Access and Parking

- 8.29 The site is situated less than one kilometre from both Reedham and Coulsdon Town Railway Stations, and the local high street which has multiple bus routes. The PTAL rating for the site is 3.
- 8.30 The London Plan suggests that car parking standards for a residential development in this location should provide a maximum of 1.5 parking spaces for 3 bedroom units and less than 1 parking space for 1-2 bedroom units. Therefore the proposed units would have a maximum standard of less than 1 parking space. It is important to note that these are maximum levels and policies seek to reduce car parking levels – thereby reducing private car trips and encouraging more sustainable modes of travel (including walking, cycling and use of public transport. A reduction in the proportionate availability of car parking spaces associated with the current proposals is considered acceptable.
- 8.31 A parking survey has been undertaken in order to assess the existing parking stress in the area of the proposal. The survey results show an existing spare capacity and a parking stress of approximately 30.9% in the vicinity.
- 8.32 The scheme provides 6 off-street parking spaces located at the front of the site, Close which would equate to a 0.66 parking spaces per unit. This is considered adequate, taking into account the PTAL, London Plan maximum parking levels and the existing low parking stress in the area.
- 8.33 The parking layout would have a suitable gradient of 1:12 and would ensure vehicles can access and exit in forward gear and would not compromise highway safety. Details of visibility splays, sight lines and retaining walls will be conditioned.

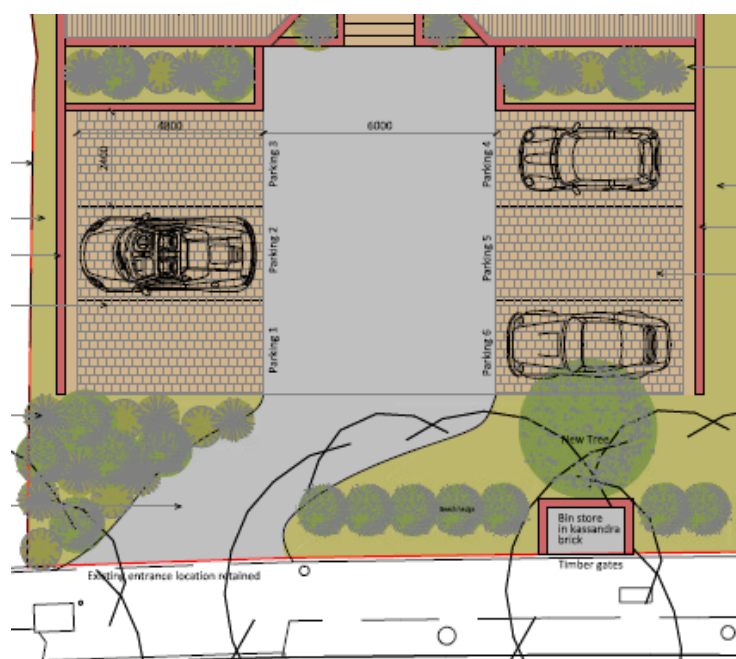


Figure 7: layout of car parking area

- 8.34 Given that there is existing hardstanding on the frontage used for parking the scheme would not be out of keeping with the surrounding location and large amount of soft landscaping is proposed to soften the car parking area and replace existing shrubbery.
- 8.35 In compliance with the London Plan, electric vehicle charging points should be installed in the parking area and this can be secured by way of a condition.
- 8.36 The capacity of the cycle storage facilities would comply with the London Plan (which would require 16 spaces) would be provided within the envelope of the main building, at lower ground floor level.
- 8.37 The bin store would be located at street level, and would be built into the side of the berm landscaping, with timber gates. A condition will be applied to ensure that adequate provision is made.



Figure 8: Side elevation showing parking area levels

- 8.38 A Demolition/Construction Logistic Plan (including a Construction Management Plan) will be needed before commencement of work and this could be secured through a condition.

Environment, Flooding and sustainability

- 8.39 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 8.40 The site is noted to be in a Critical Drainage Area at risk of flooding once in every 1000 years from surface water. The applicants have submitted a Flood Risk Assessment (FRA). To mitigate any residual risk of flooding, the FRA indicates that a suitably sized soakway would collect any surface water. In addition, the applicant has proposed permeable materials and soft landscaping. These measures would again be conditioned accordingly.
- 8.41 Given the areas of hardstanding to be utilised as parking areas, permeable paving system is proposed. This has been duly conditioned.

Trees and landscaping

- 8.42 There are no trees on site subject to a tree preservation order. The existing site has an existing tree (Horse Chestnut) at the frontage which will be removed to facilitate the development. This tree has significant cavities and limited life expectancy. The tree will be suitably replaced. Given that this tree is not protected, its proximity to the existing dwelling and its low quality and amenity value, officers have no objection to the loss of this tree.
- 8.43 There is a street tree at the front of the property. The applicant has submitted an Arboriculture Report and Impact Assessment Measures have been proposed to ensure the siting of the proposed development and the construction will not harm the street tree. This will be suitably conditioned.
- 8.44 Young trees and shrubs at the rear of the site will also be removed to facilitate the development; however replacement planting will be provided as shown.
- 8.45 The property has a front boundary wall which will be lowered to facilitate the development, and mature planting and shrubs are proposed.
- 8.46 The existing garage and associated hardstanding will be removed at the front and replaced with soft landscaping. The existing hedge (Cherry Laurel) on the eastern boundary will be pruned back and retained. The proposal would not impact on the street trees to the front of the property.
- 8.47 Retaining walls will be required on the site boundaries due to the change in levels, which blend in with the surrounding landscaping. Details of these walls will be secured by condition.
- 8.48 The single retaining wall at the building facade has been replaced with a low wall with integral planters between the amenity space & the parking. In any case, a detailed landscaping scheme will be conditioned.

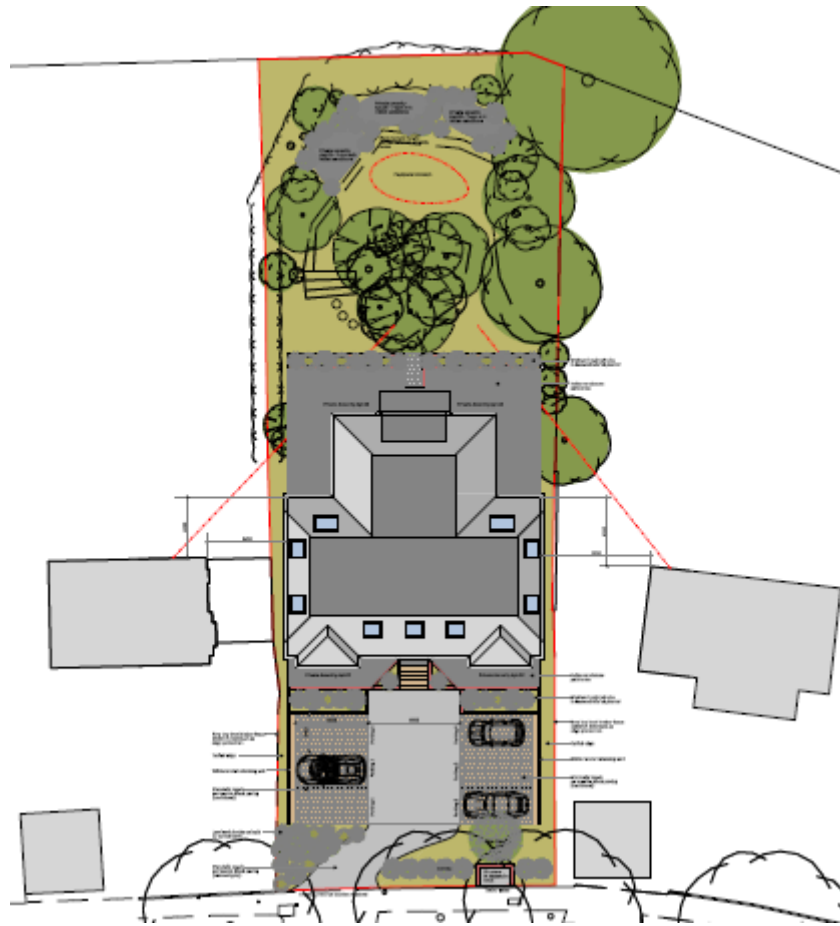


Figure 9: Proposed development and Landscaping Scheme

8.49 With regard to additional wildlife concerns, it is recommended for an informative to be placed on the decision notice to advise the applicant to see the standing advice by Natural England in the event protected species are found on site.

Conclusions

8.50 The principle of development is considered acceptable within this area. The proposal would result in the redevelopment of an existing site to provide a high quality homes. The development would be in keeping with the character of the area, and subject to the provision of suitable conditions the scheme is acceptable in relation to residential amenity, transport, sustainable and ecological matters. Thus the proposal is considered in general accordance with the relevant policies.

8.51 All other relevant policies and considerations, including equalities, have been taken into account.